



# North Fork Peachtree Creek Greenway Comprehensive Study

**FINAL Date: 12/17/2021**  
**Prepared for: DeKalb County, GA**  
**Prepared by: PATH Foundation**



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# North Fork Peachtree Creek Greenway Comprehensive study

Final Draft date:  
December 17, 2021

Prepared for:



DeKalb County  
G E O R G I A

Prepared by:





# Acknowledgment

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KAIZEN Collaborative



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01

# Overview



# 01 Overview

## Introduction

Following the one-mile-long trail and three-mile underway in Brookhaven, the North Fork Peachtree Creek Greenway Study compounds the desires of residents, City and DeKalb County officials, businesses, and non-profits to expand the Peachtree Creek Greenway (PCG) into Chamblee and unincorporated DeKalb County. This planning study will jump-start DeKalb County's opportunity for regional connectivity and continue the vision of Peachtree Creek Greenway, Inc. (PCGI) for connecting all communities along the North Fork Peachtree Creek. PCGI has been advocating for multi-use trails along the entire 12-miles of the North Fork Peachtree Creek since 2013.

The study will extend from Clairmont Road through Spaghetti Junction to Graves Park in Gwinnett County, envisioning the path eventually spanning through six jurisdictions, connecting many neighborhoods to destinations and nature. The expansion of PCG will cater to and enhance not only its immediate community, but eventually areas beyond as part of a larger network of multi-use trails, connecting into Atlanta BeltLine, PATH400, and Gwinnett County trail system developments.

DeKalb County has partnered with the PATH Foundation to continue implement multi-use trails across the County. PCGI brought to the collaboration their array of skills and valuable knowledge of the corridor and communities. The PCG expansion with its significant role of regional connectivity will be yet another product of the partnership forged between PATH, PCG Inc., Chamblee and DeKalb County.



*PATH team member descends beneath I-85 Access Road in order to field verify the feasibility of the proposed alignment.*



*A Box Turtle, discovered by the PATH team out in the field happily traverses the future corridor*



### Trail Study Process

The trail study process begins with data collection and the organization of a working group to guide the PATH planning team. The PATH team reviewed planning documents, performed extensive field research, and vetted their findings with the working group and key constituents. The N. Fork PCG Comprehensive Study incorporated input from the working group and general public through a public engagement meeting at key points in the five-month study process. Utilizing one public meeting with both in-person and virtual format, community members and stakeholders had the opportunity to review and provide feedback on the proposed trail alignment.

After three meetings with the working group, two meetings with Georgia Department of Transportation, and one public meeting, PATH team presented the draft study document, which included an implementation strategy, a timeline, trail branding and design standards. Feedback and comments were collected by an additional meeting with the working group, resulting in final revisions to the North Fork PCG Comprehensive Study.

This study will serve as the blueprint for multi-use trail development through Dekalb County with an implementation goal to build the approximately 8 miles of new trails in the next 10 years. The complete greenway will connect the Peachtree Creek Greenway in Brookhaven to Graves Park in Gwinnett.

### Working Group

PATH established a working group to guide the PCG Comprehensive Study. The formation of the working group includes:

- Dekalb County Department of Transportation
- Dekalb County Department of Public Works
- Dekalb County Commissioners and Representatives
- City of Chamblee City Council Member and Representatives
- Peachtree Creek Greenway Inc.

*(A list of individual Working Group members is included in Appendix 1.0)*

Four meetings were held between PATH and the working group via zoom during the five-month collaborative process. The working group collaborated with the PATH team to establish the overall goals for the study, provide feedback towards preliminary findings, aid in public outreach, and review planning decisions. A draft study report was circulated to collect feedback from the working group at the end of November prior to incorporating final revisions to the document.



*The PATH team conducted field work both around and through the box culvert under Spaghetti Junction*

Creating a regional transportation/recreation **greenway park** along the North Fork Peachtree Creek, connecting communities to **nature, parks, offices, and shopping centers**

### Study Goal

After the working group was established, the members spent the first meeting establishing a goal for the project. The goal of the N. Fork PCG Comprehensive study is, to create a regional transportation/recreation greenway linear park along the North Fork Peachtree Creek, connecting communities to nature, neighborhood parks, offices, restaurants and shopping centers. The trail will primarily develop as a greenway with limited amounts of necessary buffered sidepath between desired destinations.

### Data Collection and Field Work

The PATH team used the goal of the study to initiate what data needed to be gathered. Using the Dekalb County GIS data, assessment of current and future planning and development efforts, as well as feedback from the working group, PATH's analysis of existing and proposed trail connections through Dekalb County focused on the following criteria:

- Feasible for construction
- Appeal to all users
- Safety of cyclists and pedestrians
- Connections to desirable destinations
- Connections to existing trails

Over a five-month period, the PATH team met the criteria above after conducting field work and analyzing data to fully identify trail opportunities and challenges. Field work also allowed for additional trail opportunities to be found, like spur trails and alternate routes. At the end of the field work and data collection process, the PATH team worked to determine if the proposed trail routes were feasible, appealing, safe, and served the goal of the study.



*PATH and PCGI representative kick off the N Fork PCG Study Public meeting. Those in attendance included community members and leadership from both the City of Chamblee and Dekalb County*



*Members of the PATH team collaborate with community members over the proposed trail route. In-person attendees were encouraged to put a red dot on the board that highlighted where they lived and where they wanted the trail to connect to.*



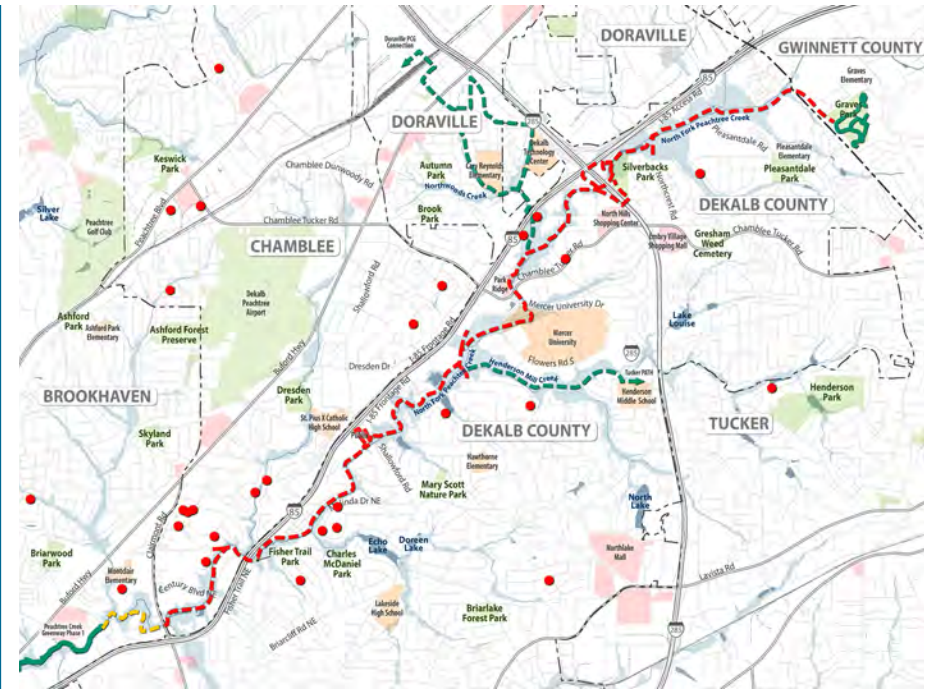
## Major Stakeholder Meetings

After identifying proposed trail alignment and presenting the preliminary findings to the work group, the team met with Georgia Department of Transportation Bridge Office and District Office to further vet the proposed trail options with interaction of GDOT right-of-way, including the identified options through Spaghetti Junction. Dialogue between the team and GDOT staff engineers provide essential feedback on the high-level planning of the trail to overcome major barriers like Interstate 85 and 285. GDOT received the proposal positively for the trail opportunities associated with I-85 while providing guidance on additional study needed to further determine trail option through Spaghetti Junction. Refer to Chapter 2 – Spaghetti Junction Path in this study for more details.

## Public Engagement

The PATH team, along with PCG Inc. and DeKalb County hosted a public meeting on October 13th at Highwoods Auditorium in the City of Chamblee. The meeting, hosted in both virtual and in-person format, provided participants with information about the study goals, the opportunities the proposed trail will bring, and an overview of the proposed trail alignments.

Involvement of the public was a key goal of the public meeting. The second half of the meeting focused on gathering feedback from the community and discussing the details of the plan. A live question and answer session concluded the meeting, followed by an open house discuss with display boards of the proposed plan. Comment cards distributed to the attendees before the meeting were collected, compiled into a summary, and presented to the working group. The meeting was recorded and live streamed via zoom and posted on the Dekalb County website to gain additional feedback and to allow a chance for those that could not attend in person to provide their input.



The map displayed above shows markups provided by the meeting attendees showing where they live and where the desired destinations are within the study area. Specific desires such as spur connections to Cravey Trail NE and McJenkin Drive NE were communicated. These input and feedback from the public helped shape the study to provide direct connections to desired destinations and neighborhoods.





02

## North Fork PCG Trail Study



# 02 North Fork PCG Trail Study

## Overview

Based on the study goal established by the working group and extensive field research, PATH team has identified 9.4 miles of multi-use trail and neighborhood connectivity opportunities that will help the Peachtree Creek Greenway expand its regional connectivity and provide an engaging trail system to achieve the overall goal.

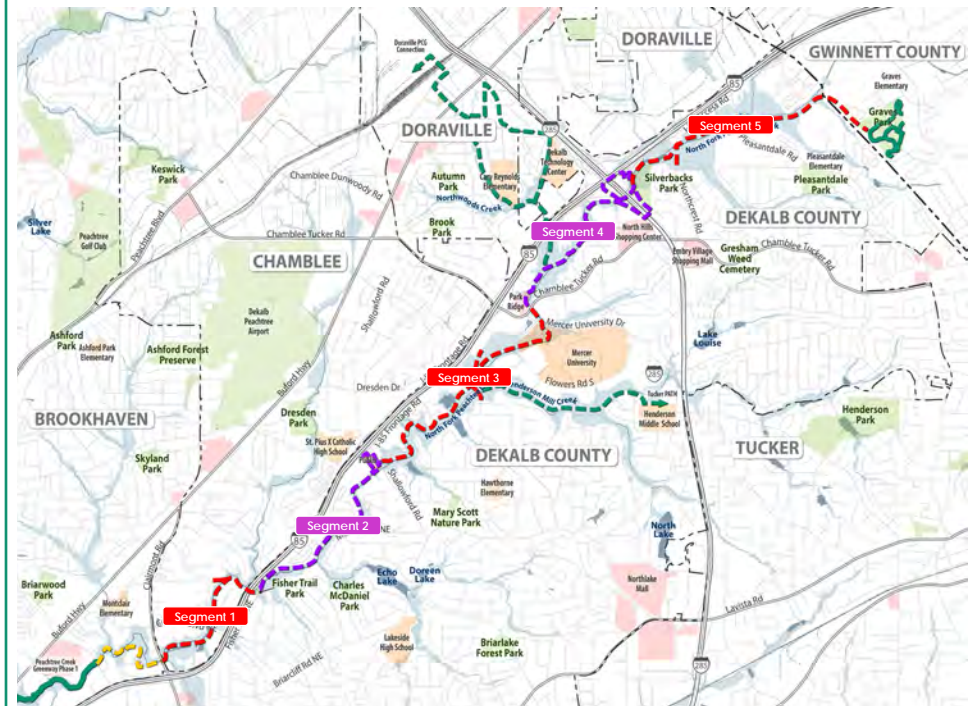
The development of the N Fork PCG has taken the value of regional connectivity into consideration due to the countless opportunities that future users of the greenway may have. The possibility to live in Dekalb County and have access to a multitude of business centers, restaurants and communities without the use of a car should be the fuel that keeps the momentum of this study going. The complete trail system will safely connect the many communities along the North Fork Peachtree Creek to parks, transit, local businesses, attractions, and regional connectivity networks such as PATH400 trail to Buckhead, Atlanta BeltLine, and Gwinnett County trail system.

Within the overall trail study, the proposed alignment is divided into six (6) segments as listed below. The adjacent map shows the beginning and end of each segment. The following pages will present each trail segment in more detail, including opportunities as well as potential challenges that will likely affect decisions regarding the priority of implementation. A detailed map for each segment is included with existing images, as well as before and after vision graphics of selected locations along each proposed trail segment.

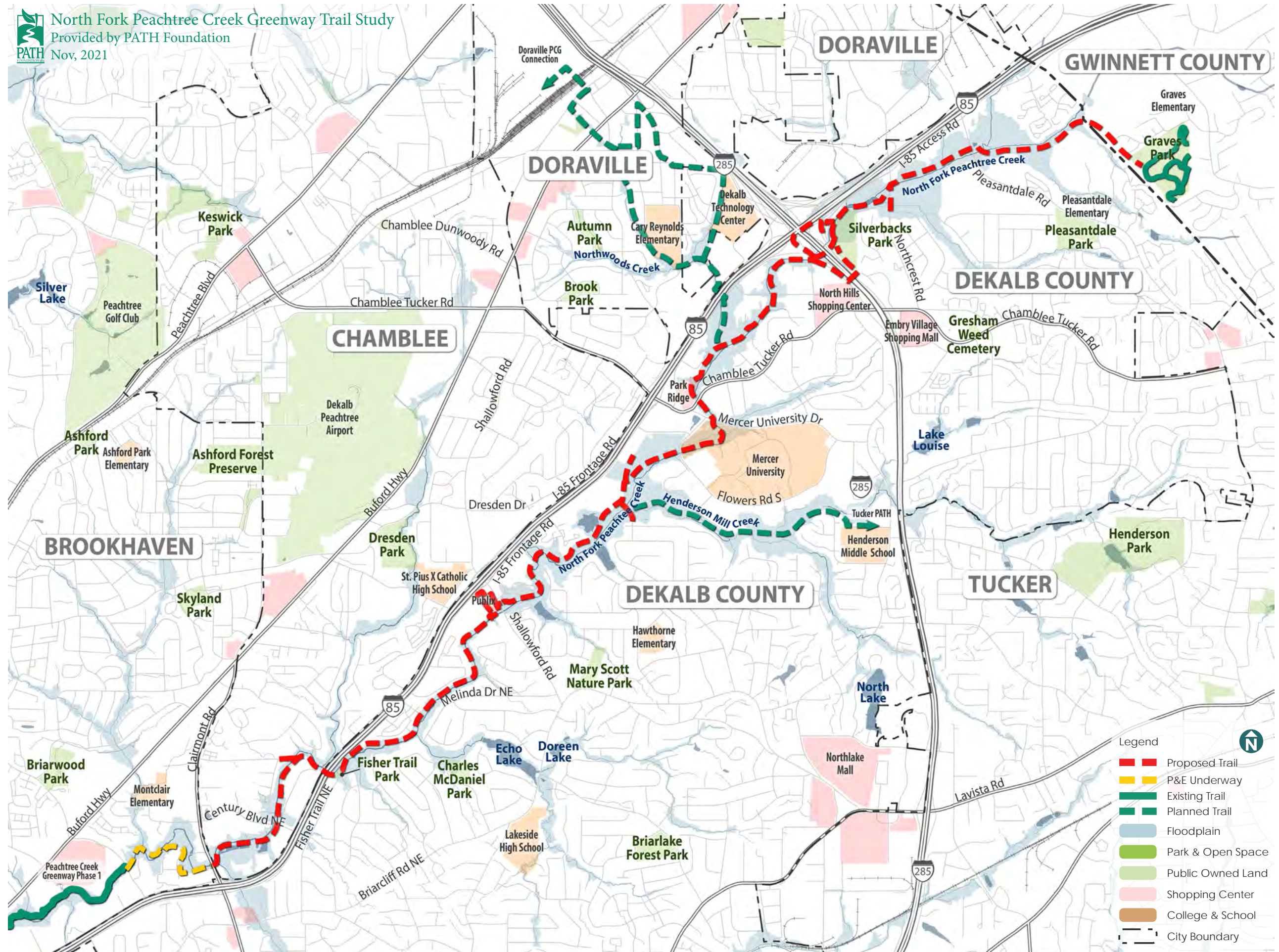
## Trail Segments

1   Clairmont Road to Fisher Trail Park	10
2   Fisher Trail Park to Shallowford Road	16
3   Shallowford Road to Chamblee Tucker Road	20
4   Chamblee Tucker Road to Presidential Parkway	24
Spaghetti Junction	28
5   Silverbacks Park to Graves Park	32

## Proposed Trail Segment Map









# Segment #1 | Clairmont Road to Fisher Trail Park

## Description

Segment #1 begins at Clairmont Road where the underway Peachtree Creek Greenway phase 3 will run under the road bridge and terminate at the Brookhaven/Chamblee city limit. Beginning on the south side of the creek, the trail will travel behind Sam's Club, U-Haul, and the Marriott before reaching Century Boulevard. This segment of trail will utilize non-buildable land within floodplain/floodway between the creek and three major detention ponds and will immediately immerse cyclists and pedestrians into nature.

The trail will travel under Century Boulevard and occupy both the available greenspace and sewer easement that follow the western boarder of Century Center. A future creek crossing at the north end of the campus will give rise to a desired neighborhood connection at McJenkin Drive.

From Century Center, a proposed underpass will create a safe, quality connection for trail users who once dreamed of an easier way to cross I-85. Segment 1 will end at Fisher Trail Park, offering a connection to the county-owned greenspace, while providing a neighborhood connection for residents of a large community sitting between Briarcliff Road and the interstate.

Upon the completion of PCG Phase III in Brookhaven, this segment will carry the Peachtree Creek Greenway through the City of Chamblee and into Dekalb County.

## Segment #1 Overview

**Connecting Destinations:** Future PCG in Brookhaven, Sam's Club, Marriott Hotel, Century Center, South College Atlanta, Fisher Trail Park

**Distance:** 6,392 Linear Feet (1.2 miles)

## Opportunities and Benefits

- Opportunities for shared parking space at Sam's Club and Century Center
- Connection to Century Center as a large employment center
- Connection to large residential community within unincorporated Dekalb County
- Minimal acquisition from large properties
- Received support from major stakeholder for donated easement

## Potential Challenges

- Requires encroachment permit for trail under I-85 and frontage roads within GDOT right-of-way.

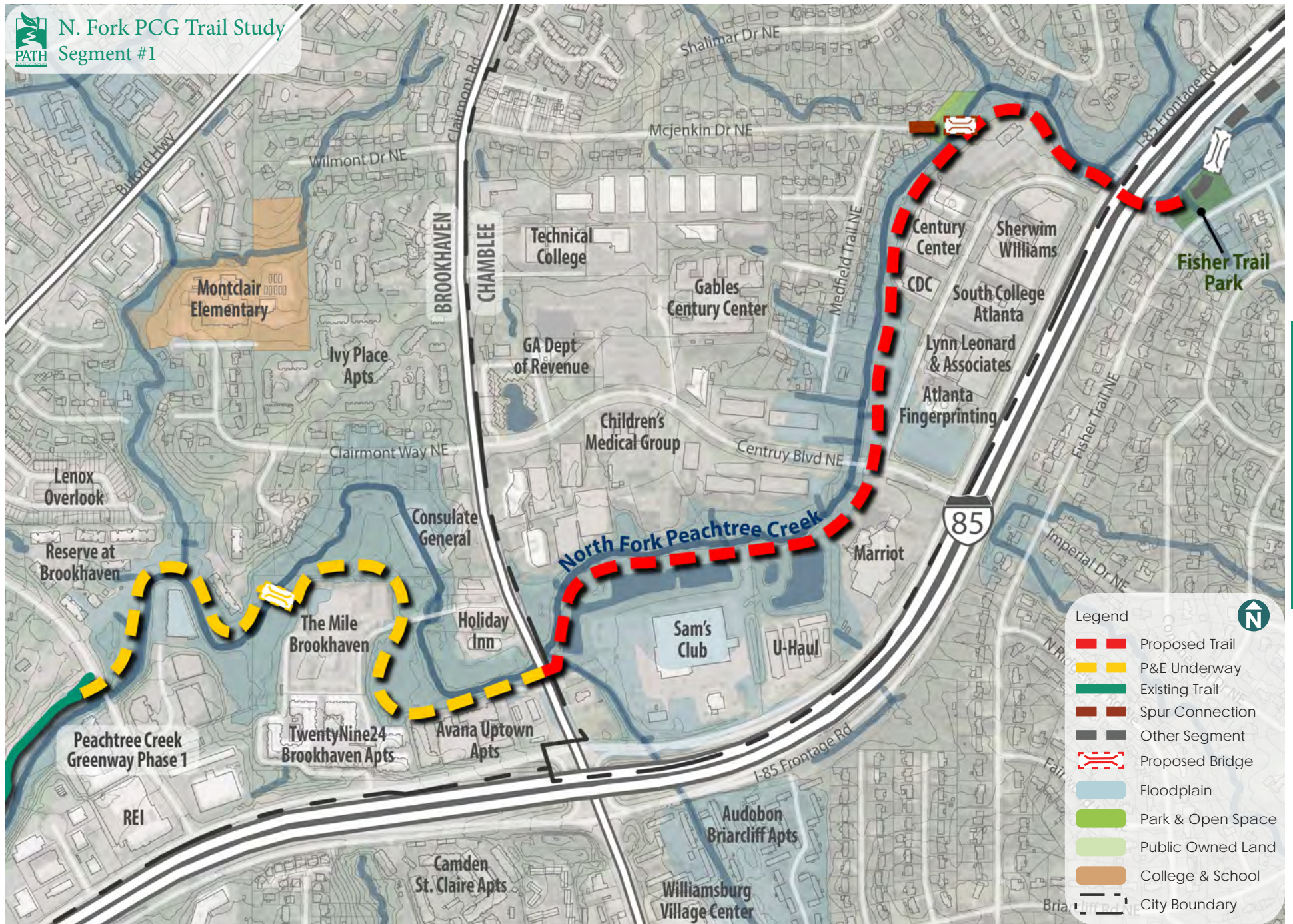
## Trail User Scenarios

- Students commuting to South College Atlanta and Technical College System of Georgia.
- Employees of the nearby business parks at the Chamblee/Tucker boarder may have an alternative option for commuting in to work.
- Residents of Dekalb County gain regional multimodal connectivity toward the city of Brookhaven and the City of Atlanta.





N. Fork PCG Trail Study  
Segment #1







## Segment #1 | Century Boulevard Underpass

The vision graphic below depicts a greenway trail under Century Boulevard behind the Marriott hotel. An existing pipe just outside the right-hand side of the image will be channeled under a structural slab. This underpass separates cyclists and pedestrians from vehicular traffic and connects users to the Century Center campus.



Existing conditions of the N Fork Peachtree Creek under Century Boulevard, facing north





## Segment #1 | Existing Conditions



Existing sewer easement adjacent to Century Center will allow for a neighborhood access point connecting to McJenkin Drive



Existing drainage structure along sewer easement to the north of Century Center





## Segment #1 | Trail under I-85 and Access Roads

The illustration below shows an elevated trail sections with pier structures as the alignment navigates under I-85 towards Fisher Trail Park. Trail lighting, security, and railings will ensure a safe experience for users. The opportunity for artistic expression of existing highway structures could enhance the visual quality of the greenway.



Existing conditions of I-85 the Access Roads, facing west



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# Segment #2 | Fisher Trail Park to Shallowford Road

## Description

Segment 2 will provide trail access to seven (7) apartment/townhome complexes and multiple neighborhood areas. The trail takes off from Fisher Trail Park with a bridge crossing to the north side of the creek. Available greenspace adjacent to I-85 will give way to the trail as it returns to grade. The trail then follows the sewer easement along the creek towards east and connects behind several multi-family complex.

An existing parcel owned by Dekalb County offers the opportunity for an additional spur connection across the N. Fork Peachtree Creek to Nancy Lane NE. The single family neighborhoods in this area north of Echo Lake will have direct access to the PCG.

The terminal point of segment 2 is Shallowford Exchange shopping center along Shallowford Road. Trail users will have access to a variety of amenities, most notably Publix. In order to reach the shopping center, the trail will run under the existing driveway bridge within the Brooke Apartments and immediately increase elevation via a switchback system with retaining walls.

Once at grade with the shopping center, there are two possible alignments that will finally connect users to Shallowford Exchange. The first option sees the trail occupying a strip of greenspace between Shallowford Exchange and the adjacent multi-family property, “The Brooke by Arium,” to the west. This alignment will align with the northern perimeter of the existing parking lot and climb up to Shallowford Road to the existing signalized intersection. The second option occupies an existing delivery/fire access lane and falls adjacent a large gabion wall. Due to limited space, this alternative will be a shared condition for the trail users until they reach to a side path along Shallowford Road to the signal.

Segment 2 will terminate with an at-grade crossing from the main vehicular entrance to Shallowford Exchange, where an additional crossing can be added to a modified traffic signal to allow trail users to safely cross this busy arterial road.

## Segment #2 Overview

**Connecting Destinations:** Fisher Trail Park, Publix and other businesses at Shallowford Exchange

**Distance:** 8,551 Linear Feet (1.6 miles)

## Opportunities and Benefits

- Connects multiple residential areas and multi-family housing properties to the Greenway
- Access to Fisher Trail Park to the south and Shallowford Exchange to the north
- Existing sewer easement provides ample space for greenway trail
- Connection to a shopping center

## Potential Challenges

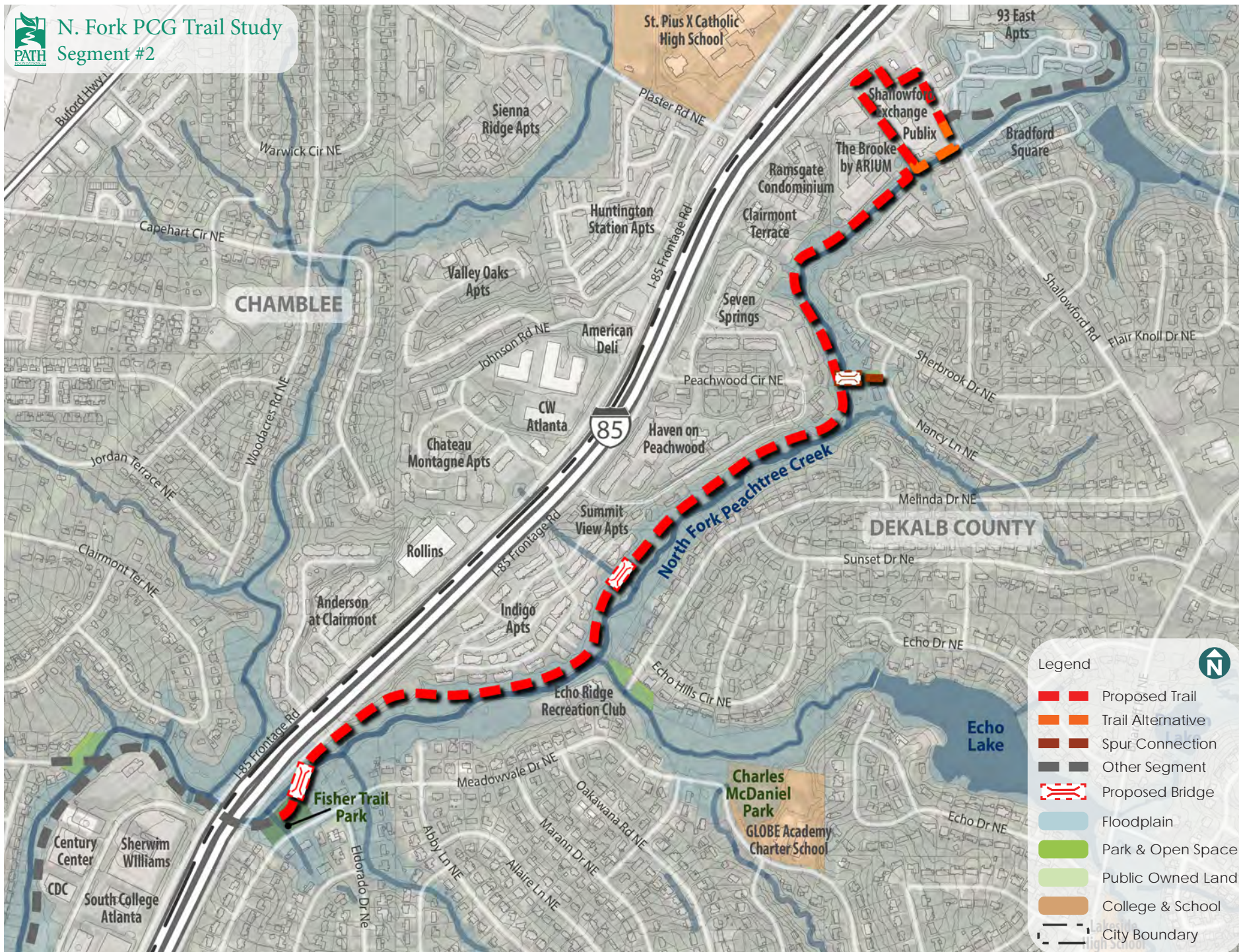
- Requires easement acquisition from multiple properties including several residential properties
- Requires further coordination with large property owner and stakeholder to determine feasible alignment option
- Identifying possible temporary terminations to break the segment into shorter phases for ease of implementation

## Trail User Scenarios

- Neighborhood residents accessing the trail for commuting, fitness or recreation.
- Residents along the trail will have the opportunity to access Publix and other businesses at Shallowford Exchange without having to drive along I-85 Frontage Road or other busy connecting roads.



**N. Fork PCG Trail Study**  
Segment #2







## Segment #2 | Existing Conditions



Existing conditions of the N Fork Peachtree Creek on the south side of I-85 looking south east. A trail bridge will connect the trail to Fisher Trail Park and the surrounding residential area.



Existing sewer easement between the creek and many multi-family housing properties along I-85 Frontage Road provide the required conditions for a greenway trail connection.



## Segment #2 | Existing Conditions



Trail underneath Shallowford Road bridge will not be feasible due to the built-out existing conditions on both sides of the creek. The image above shows an existing gabion wall adjacent to Shallowford Exchange along the Publix shipping & receiving area along and fire access lane, facing north west.



Existing gabion wall and box culvert under Shallow Ford Road from Shallowford Exchange, facing north



# Segment #3 | Shallowford Road to Chamblee Tucker Road

## Description

Beginning with the at-grade crossing of Shallowford Road, Segment 3 continues to travel north by taking advantage of available greenspace within the 93 East Apartments and Georgetown of Atlanta Condominium properties. Direct access and security measures can be provided to residents for ease of access to the greenway.

This alignment will provide additional neighborhood access via two small bridges that will cross the North Fork Peachtree Creek and Henderson Mill Creek. From there, a further connection is planned to follow Henderson Mill Creek to tie into the planned Tucker PATH, providing further regional connection into Dekalb County, the Northlake CID area, and City of Tucker.

As Segment 3 continues north, users will have access to the Regal Hollywood Cinema, and a spur connection will connect to the future redevelopment along I-85 Frontage Road.

From the cinema, the trail follows the creek to the east towards Mercer University. This section of Segment 3 will consist of top-down boardwalk to limit disturbance of the wooded areas between Flowers Road and I-85 Frontage Road.

A proposed underpass of Flowers road will connect the trail to Mercer University's campus and will lead to an at-grade crossing of Mercer University Drive. This trail section can be further coordinated and incorporated within Mercer University's future development within the school property to enhance overall connectivity to the campus.

A five-box culvert is then the preferred option for the trail to cross under Chamblee Tucker Road. A flood study will be required in order to confirm the feasibility to use the eastern-most culvert for the trail. Segment 3 terminates with a connection climbing up from the box culvert to the Park Ridge 85 Office Park shopping center and multi-family property on the north side of Chamblee Tucker Road.

## Segment #3 Overview

**Connecting Destinations:** Publix at Shallowford Exchange, Regal Hollywood Cinema, Mercer University, Park Ridge 85 Office Park, Ponko's Restaurant, MARTA Bus line to Chamblee Transit Station

**Distance:** 12,519 Linear Feet (2.4 miles)

## Opportunities and Benefits

- Connects multiple residential areas and multi-family housing properties to the Greenway.
- Connects to major employment center on Chamblee Tucker Road
- Opportunities for multiple neighborhood connections
- Existing sewer easement provides ample space for greenway trail
- Establishes a critical connection to Mercer University
- Regional connectivity opportunities to Northlake CID area and Tucker
- No easement acquisition from individual residential properties

## Potential Challenges

- Requires further studies on hydrology to determine feasibility of trail underpass Mercer University Drive and Chamblee Tucker Road
- Identifying possible temporary terminations to break the segment into shorter phases for ease of implementation

## Trail User Scenarios

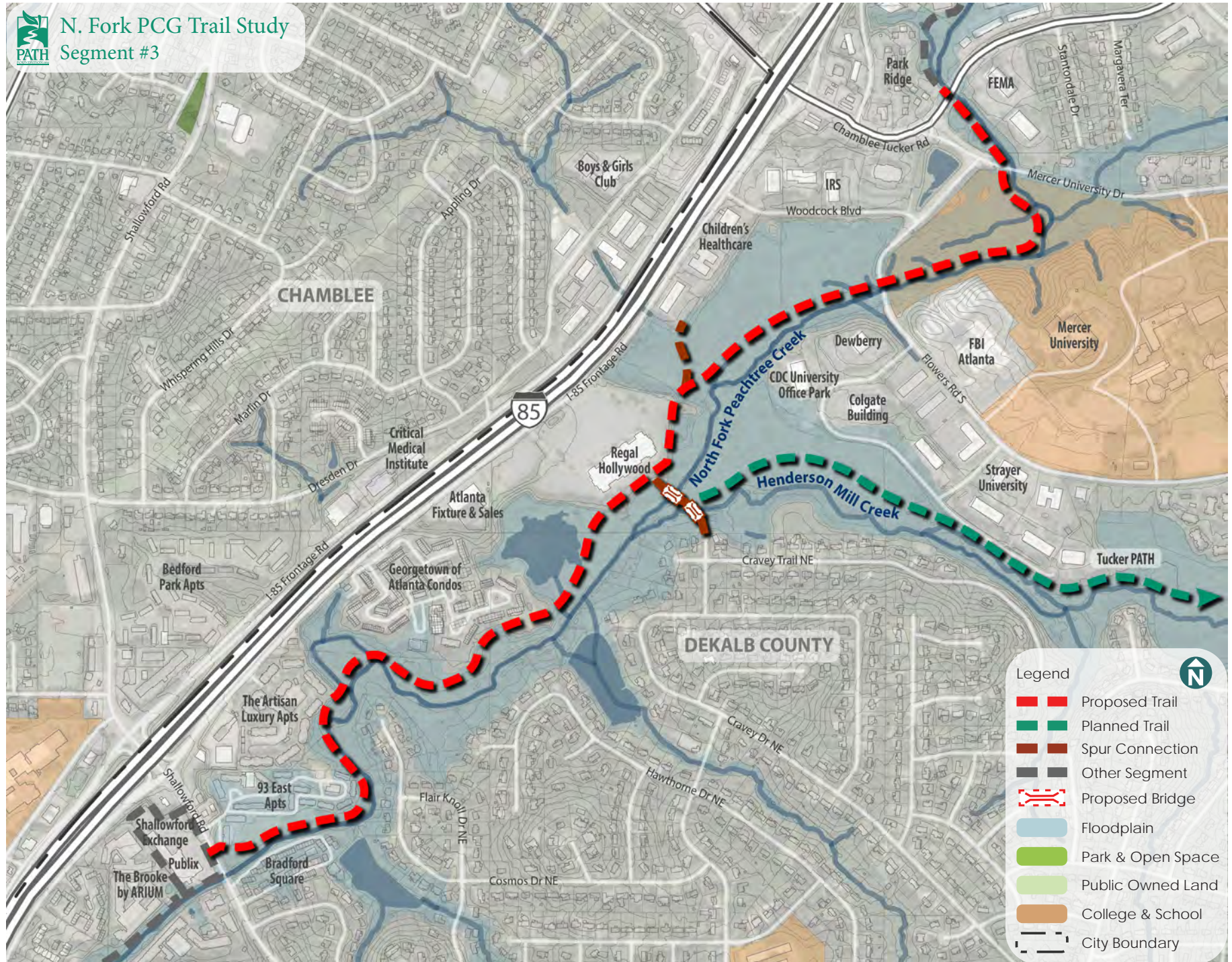
- Students of Mercer University and Strayer University will have the option to commute to class via the greenway.
- The expansive connection between Shallowford Road and Chamblee Tucker Road provides residents with access to shopping, doctors offices, and recreational opportunities.





# N. Fork PCG Trail Study

## Segment #3







## Segment #3 | Greenway Trail at 93 East Apartments

The below vision graphic depicts a greenway trail navigating the eastern edge of the 93 East Apartments along the N Fork Peachtree Creek. The trail will provide access for local residents to walk to Shallowford Exchange without overly impacting the existing fields on the property. Current signs of erosion on the creek side show that erosion control measures will need to be implemented into the future trail design.

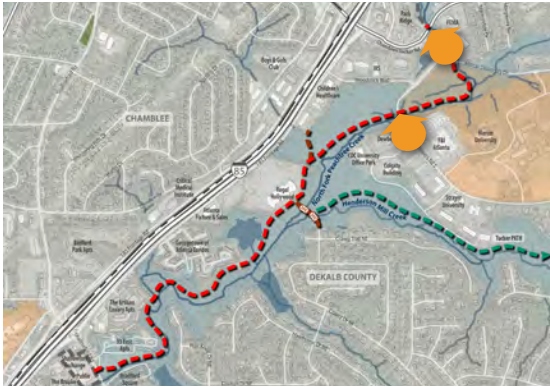


Existing conditions of the N Fork Peachtree Creek at 93 East Apartments, facing north-east





## Segment #3 | Existing Conditions



Existing conditions under Flowers Road, facing north. 8'-0" clearance for multi-use trail is achievable under Flowers Road



Existing five box-culvert under Chamblee Tucker Road, facing north-west. A flood study is advised to confirm that the eastern most box may be reconfigured for a 10'-0' wide multi-use trail.



# Segment #4 | Chamblee Tucker Road to Presidential Parkway

## Description

Beginning at the Park Ridge 85 Office Park north of the proposed Chamblee Tucker Road underpass. Here the trail will occupy a shelf adjacent to the creek as the trail ascends to towards Marjan Drive. After spanning over a tributary, the trail continues along the existing greenspace between Marjan Drive and the creek.

The trail will cross two additional tributaries as the space between the road and the creek begins to narrow. A pedestrian bridge is proposed to be parallel to an existing road bridge over the tributary known as Stewart. Here, a planned PCG connector trail from City of Doraville will tie in.

Multiple Dekalb County owned parcels sit across the creek and could potentially provide future neighborhood connectivity to the greenway.

As the alignment approaches Presidential Drive, the amount of landscape area available diminishes to an existing gabion wall adjacent to an existing office parking lot. The proposed trail will cross the creek towards ADI and land adjacent to the building along the edge of the property before reaching the at-grade crossing of presidential Drive.

Segment 4 continues to follow the curvature of the creek as it navigates the Clyde Industries properties. The proposed trail is planned to follow the non-buildable area along the edge of the property and become a catalyst of redevelopment to revitalize this sleepy area.

The final stint of segment 4 entails an at-grade crossing of Presidential Parkway, and a greenway section traversing between the creek and the road until it divides into three optional alignment to overcome the Spaghetti Junction to Silverbacks Park. These three alternatives are outlined in detail in the next section of this chapter.

## Segment #4 Overview

**Connecting Destinations:** Park Ridge 85 Office Park, future redevelopment of Clyde Industries, North Hills Shopping Center, Doraville PCG regional connection, Stone Summit, LifeLine Animal Project, and an array of restaurants at Food Junction ATL

**Distance:** 6,014 Linear Feet (1.1 miles)

## Opportunities and Benefits

- Connection to the Park Ridge 85 Office Park and businesses along Marjan Drive
- Opportunities to influence (re)development of large underutilized commercial properties
- Explored three options to overcome the barrier of the Spaghetti Junction
- Trail will connect users to office parks and shopping centers
- Establishes future connection to Doraville across I-85

## Potential Challenges

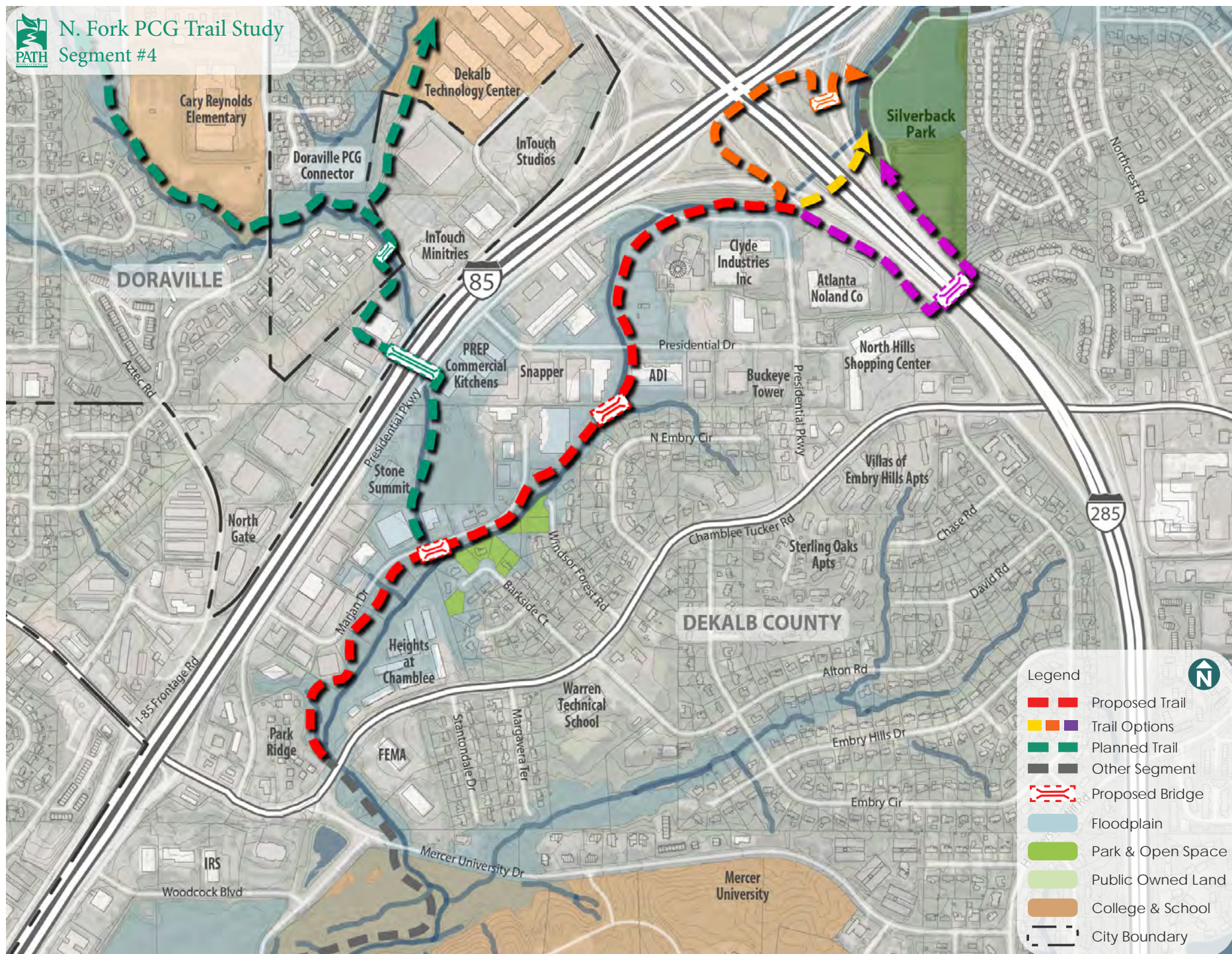
- Requires easement acquisition from multiple properties
- Requires further studies and coordination with Georgia Department of Transportation to select a feasible option for the Spaghetti Junction path; detailed summary of challenges are outlined in the next section of this study

## Trail User Scenarios

- Segment 4 will provide users access to office parks where people may work or have access to services
- The redevelopment of Clyde Industries will provide additional amenities to users and local residents



**N. Fork PCG Trail Study**  
**Segment #4**







## Segment #4 | Existing Conditions



Existing conditions along Marjan Drive, facing east. The proposed trail will sit between the existing pavement and the creek so not to interfere with existing shipping traffic. A separate trail bridge is proposed to the south of the road bridge in this image.



Existing conditions of a gabion and drainage structure, facing west. A proposed trail bridge will cross the creek and provide a safe connection to Presidential Drive.



## Segment #4 | Existing Conditions



Existing conditions of Presidential Drive, facing west. A proposed at-grade crossing will connect the trail to the future redevelopment of the Clyde Industries parcels.



Existing conditions of Presidential Parkway, facing north-west. The proposed trail will meander through the greenspace between existing concrete and the creek as it approaches I-285.

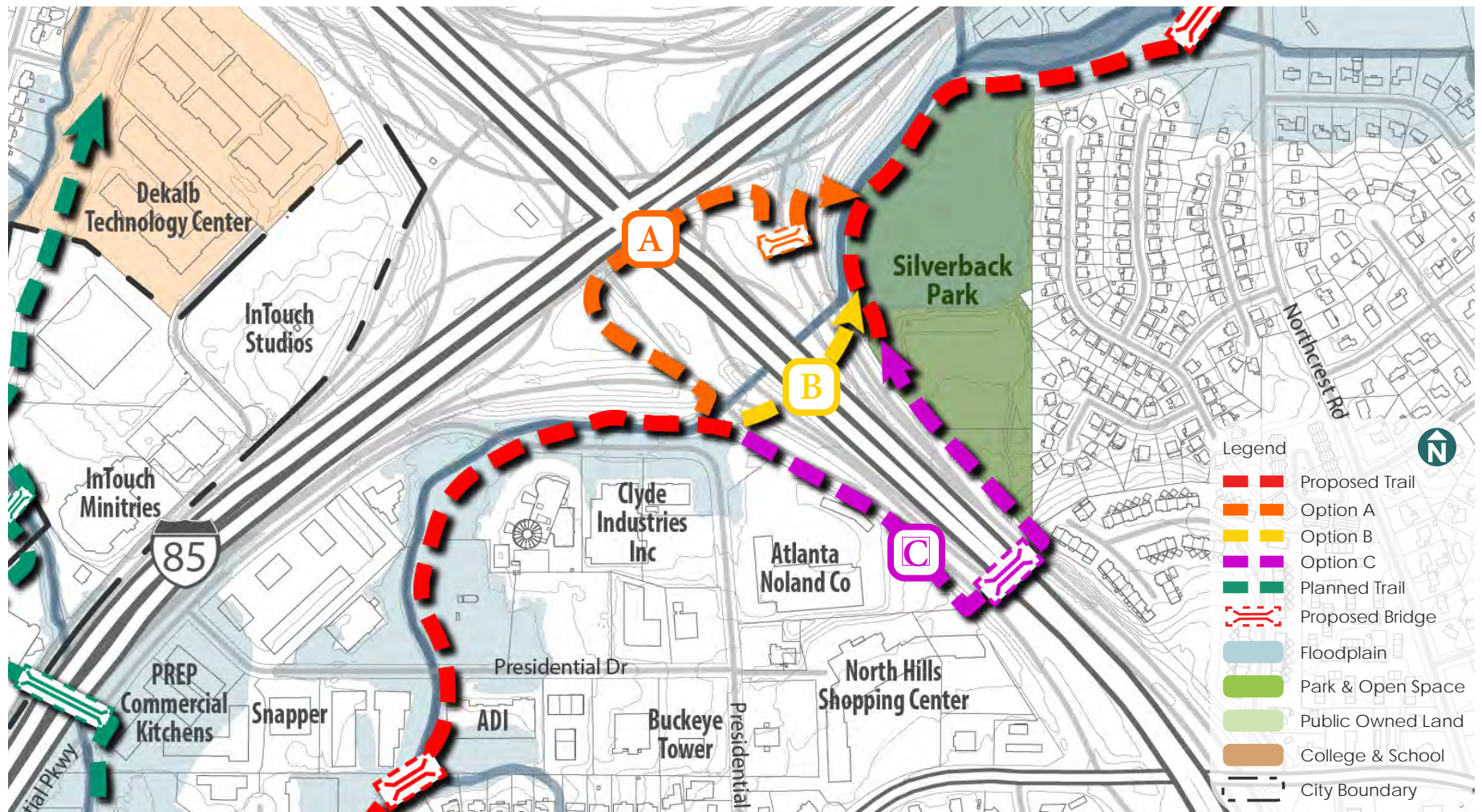


# Spaghetti Junction Trail Options

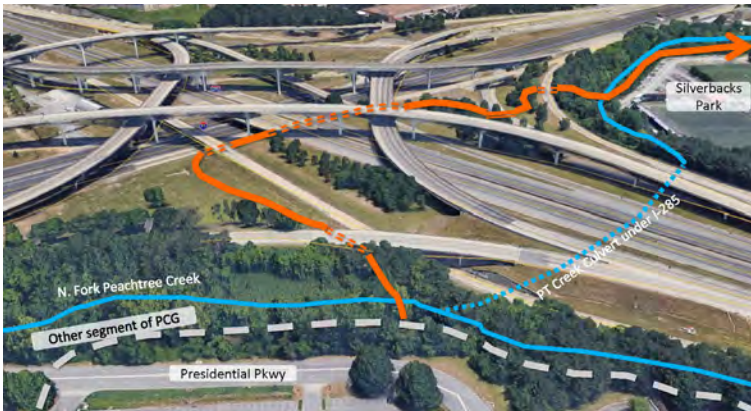
The PATH team has identified three (3) possible options for the North Fork PCG to breach the I-285 barrier. Each option requires further coordination and studies with GDOT to determine feasibility and possible implementation timeline. The three options are:

- Option A - Leapfrog
- Option B - Box Culvert underpass
- Option C - Fly Over Bridge

Two (2) coordination meetings were held between the PATH team, PCG Inc., GDOT Bridge Office, and GDOT District Office to collect initial feedback on the proposed options. The following pages provide detailed descriptions and feedback obtained from GDOT for each option.







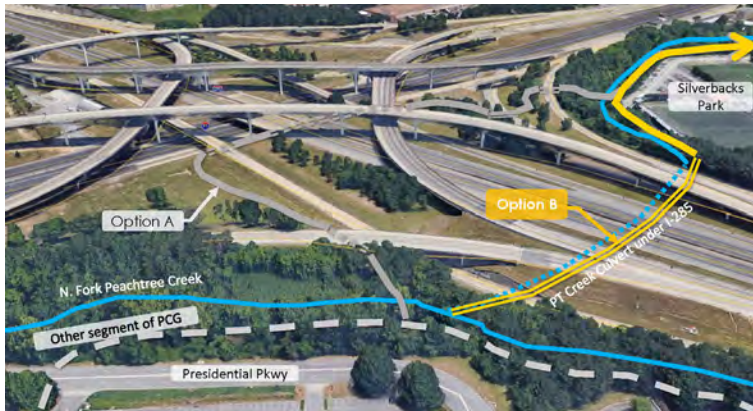
## Spaghetti Junction | Option A

Option A will take advantage of the available landscape area between access lanes. Identified as the “Leapfrog” option, a series of tunneling and bridging will add a pedestrian travel opportunity through the Spaghetti Junction. This segment will land at Silverbacks Park and will need to cross the creek to both make a connection to the stadium and continue the greenway on the south side of the greenway.



- 1) From Presidential Parkway, the alignment will tunnel under the I-285/Chamblee Tucker Road Access ramp. From there the trail will occupy underutilized greenspace paralleling I-85.
- 2) The proposed alignment runs adjacent to I-85, taking advantage of existing underpasses. This portion of Option A is the greatest pinch point due to lack of adequate space for a trail. Proximity of pedestrians to high-speed traffic could raise additional safety concerns.
- 3) A combination of bridging and tunneling will carry the alignment over/under the I-85/I-285 access ramps adjacent to Silverbacks Park





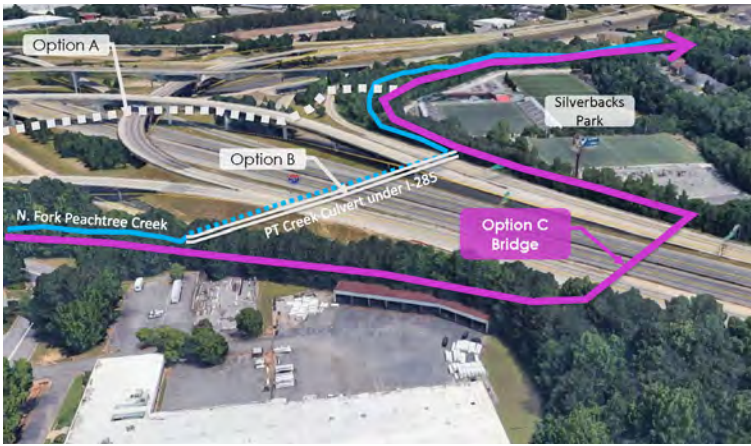
## Spaghetti Junction | Option B

This alternative will send trail users under I-285. The trail will occupy the southern-most box of a five-box culvert that runs approximately 350' under the interstate. This option has the least impact to GDOT right-of-way, therefore was the preferred option by GDOT bridge office from a planning level perspective. A hydrological study is required at this location to understand capacity with the remaining four boxes during normal rain event and 100-year flood to understand the potential impact to floodplain and hydrological functions for the trail going through the box culvert. The curving tunnel will require ample lighting and security measures to ensure that trail users feel safe as they descend beneath the interstate.



- 1) The southern opening to the box culvert shows large amount of sediment and debris that requires clean-up, which shows a sign of potential maintenance burden once the trail is built.
- 2) Members of the PATH team traversed the box culvert that would house the proposed trail. Almost 4'-0" of silt has collected along the floor of the culvert. Outside light is not visible at the center of the culvert.
- 3) The northern opening of the box culvert will require clean-up and maintenance of debris that collects during major rain events. The trail will then slope up to meet the drive and parking area of Silverbacks Park.





## Spaghetti Junction | Option C

The third alternative of crossing I-285 is approximately 360-foot long bridge spanning from Presidential Parkway to a landscape strip on the edge of Silverbacks Park. The bridge will consist of three spans, utilizing existing barriers and landscape strips to set the abutment structures. The trail will need to travel up from Presidential Parkway to achieve the 17'-6" minimum clearance above the travel lanes. This caged pedestrian bridge will also run underneath the future I-285 Top-End Express Lanes that are currently in the conceptual design phase. The greenway will land on top of an existing retaining wall along the north edge of the interstate. From here the trail will navigate Silverbacks Park as a side path as it continues to Segment 5.



1) Conceptual rendering of the future I-285 Top End Express lanes provided by GDOT. The N Fork PCG bridge will likely sit between the existing travel lanes and the future express lanes.

2) Existing conditions of Spaghetti Junction, facing north-west. The proposed trail bridge will run behind the highway signs to clear visibility of the sign, before landing at Silverbacks Park. (right)



# Segment #5 | Silverbacks Park to Graves Park

## Description

Segment 5 begins within Silverbacks Park, connecting users to the regularly scheduled sporting events held in the stadium. The trail will follow the main drive of the park before crossing to the northside of the creek via a two-box culvert at the intersection of Northcrest Road and Northeast Expressway.

A proposed neighborhood connection along the east side of Northcrest Road will connect the Rose Arbor community and single-family properties to south.

The trail will occupy an existing sewer easement and bring connectivity to more office/industrial parks before crossing under Pleasantdale Road and back to the south side of the creek. Top-down boardwalk structures are seen to be used as the meanders through large wooded areas around The Encompass Digital Media campus.

Opportunities for multi-family housing connections are plentiful as the trail follows the creek to the northeast before turning southeast towards Graves Park.

The connection into Gwinnett County will require easement from a group of private property owners between Las Palmas Apartments and Graves Park. Once on park property the trail will tie into the park's existing 12' wide paved path. The park's existing parking lot and the presumed source of the North Fork Peachtree Creek will cap this regional greenway connector.

## Segment #5 Overview

**Connecting Destinations:** Silverbacks Park, Northcrest Driving Range, Graves Park

**Distance:** 10,080 Linear Feet (1.9 miles)

## Opportunities and Benefits

- Regional Connectivity to Graves Park in Gwinnett County
- Connect numerous multi-family properties to recreational opportunities.
- Neighborhood connection along Northcrest Road

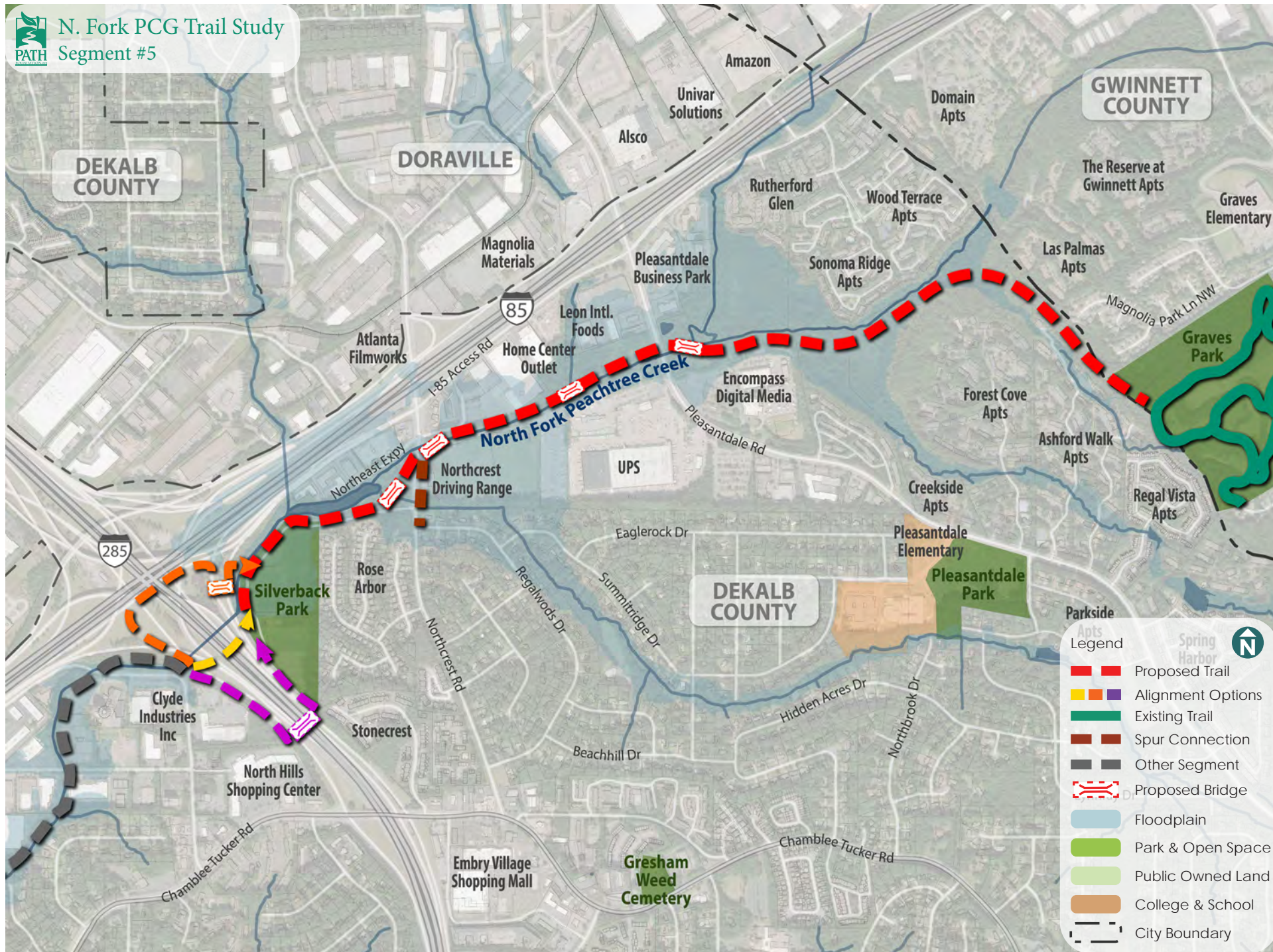
## Potential Challenges

- Requires easement acquisition from multiple properties
- Identifying possible temporary terminations to break the segment into shorter phases for ease of implementation

## Trail User Scenarios

- Families or groups can travel from the multi-family housing properties along the Dekalb/Gwinnett county line to Silverback Park and see a game without having to drive to the stadium.
- Families living along the Dekalb/Gwinnett county line may have safe, car free access to Graves Park, a major greenspace for the area.
- Regional transportation opportunity for commuters traveling to and from business and office parks along the greenway.









## Segment #5 | Pleasantdale Road Underpass

The illustration below depicts a 10'-0" wide trail on top of a retaining wall through an existing box culvert under Pleasantdale Road. Breakaway railings are proposed to prevent major damage to the structure during rain event should the railing hit by large objects. A minimum 8'-0" height clearance should be achieved under the bridge. This vision allows users to cross the wide and busy Pleasantdale road safely and with no impact to the existing flow of traffic.



Existing conditions of the three box culvert under Pleasantdale Road, facing northeast





# Segment #5 | Existing Conditions



Existing conditions of a double box culvert under Northcrest Road, facing east. The proposed trail will utilize the south (right) box to cross, similar to the proposed conditions under Pleasantdale Road.



Existing conditions of the 12' wide trail and dog park at the eastern corner of Graves Park, facing northwest. The proposed alignment will connect to graves park at or near the dog park where signage and markings will guide trail users to and from the main parking area.





03

## Implementation Strategy



# 03 Implementation Strategy

## Overview

This chapter suggests strategy with specific steps to ensure a timely and orderly implementation of this plan to capture the momentum for the *North Fork Peachtree Creek Greenway* moving from a vision to action. The following items are presented in this chapter:

- Model Project
- Cost Estimate
- Funding Strategy
- Implementation Timeline
- Formation of an Implementation Committee

## Model Project

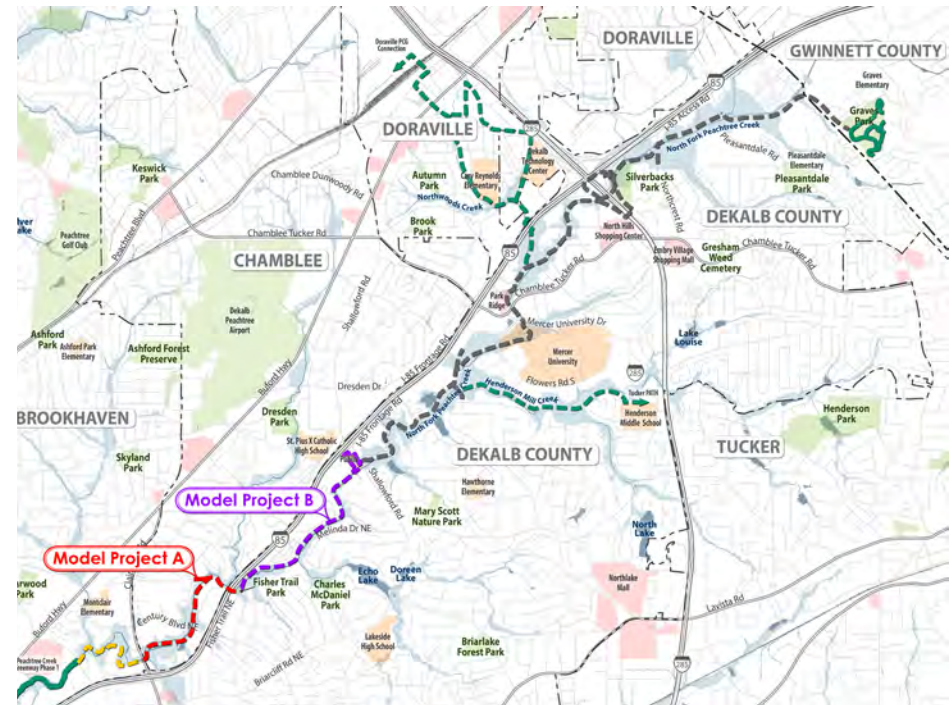
The secret to advancing a trail system is to construct a model project while the ink is still drying on the plan. The model project needs to meet a series of criteria informed by the purpose and objectives of the trail system to become highly used by the public. It requires local support and dedication to make it happen. In the meetings with the Working Group, the PATH team recommended identifying the model project of the *N Fork PCG Trail Study* for implementation in 2022/2023.

### What makes a successful model project?

- Demonstrates amenities
- Worthy stand-alone project
- Minimal acquisition
- City/County-wide support
- Control over implementation timeline

The PATH team presented the criteria for selecting a successful model project, along with two suggested model project options to the Working Group. Model Project A builds off the future PCG Phase III and spearheads the PCG's regional connection into Dekalb County. Segment 1 also requires the least amount of acquisition of all the proposed segments in this study. Model Project B will make a significant connection within unincorporated Dekalb County, linking multiple residents to destinations along the creek. Survey, engineering, and design should be initiated for the model projects as the plan is accepted by Dekalb County.

## Suggested Model Projects

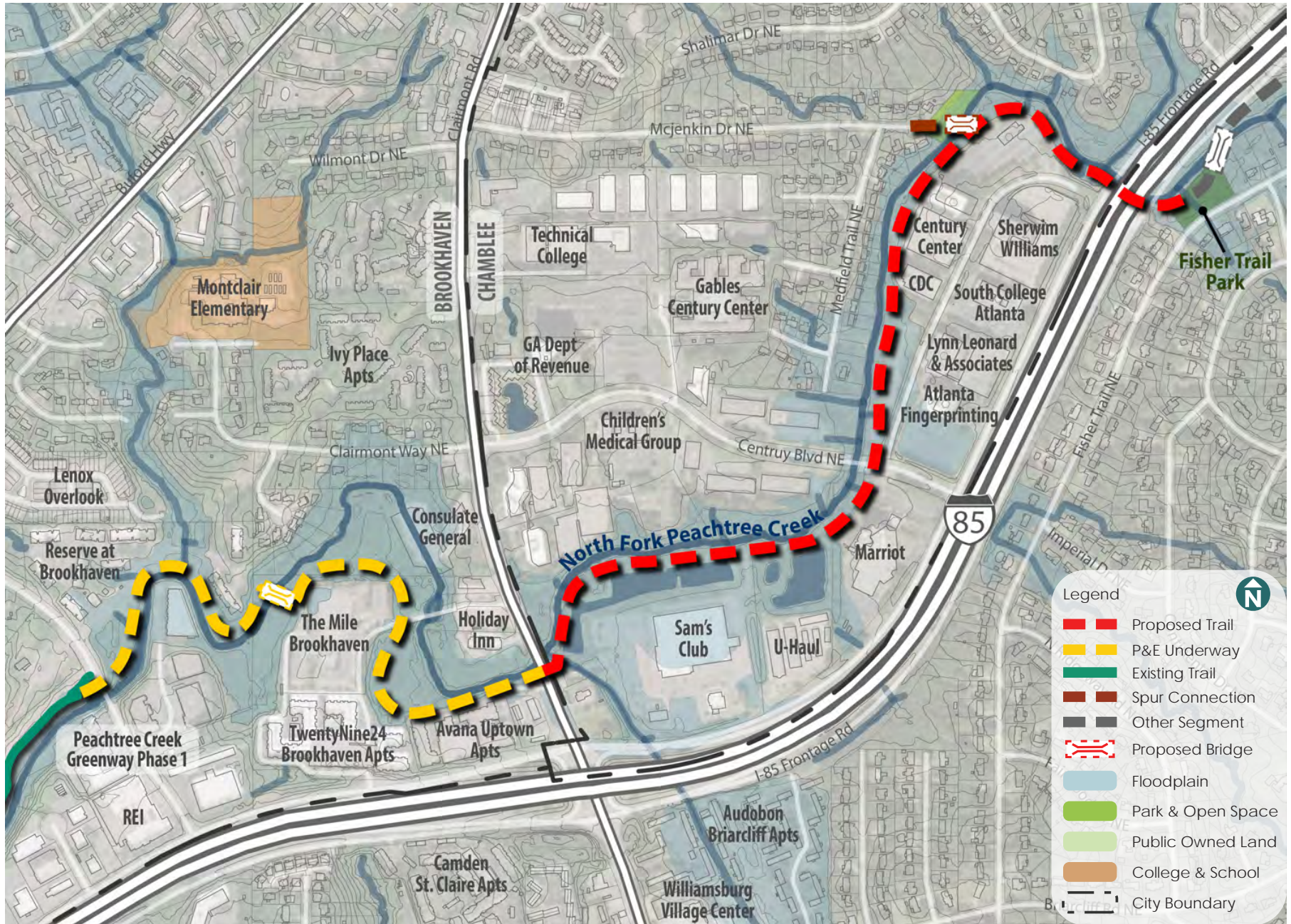


The PATH planning team has identified two potential model projects:

- Model Project A - Clairmont Road to Fisher Trail Park - \$5.7M
- Model Project B - Fisher Trail Park to Shallowford Road - \$6.3M

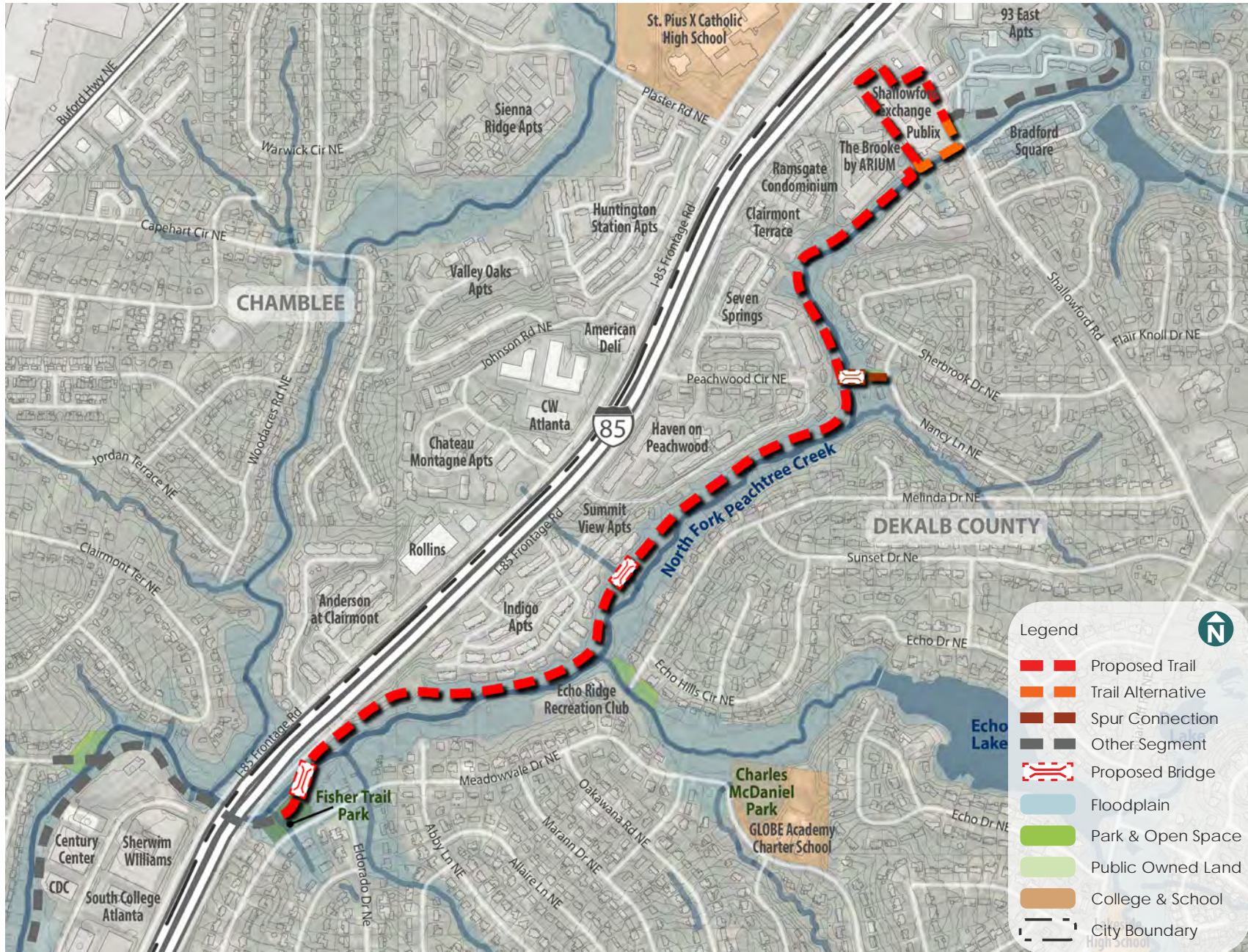


# Model Project A | Clairmont Road to Fisher Trail Park





# Model Project B | Fisher Trail Park to Shallowford Road





### Cost Summary

The below chart shows the summary of total preliminary cost estimate for each segment and alternatives identified in this study. The preliminary cost includes estimated pre-construction engineering cost and construction cost for the mainline trail. Estimated cost for easement and property acquisition are not included but should be considered prior to beginning implementation.

Estimated engineering (P&E) cost includes cost for surveying, designing for construction, permitting, bidding, and construction administration of the project.

Estimated construction cost is based on material and labor pricing from summer 2021 using Peachtree Creek Greenway trail standards, lighting & security cameras, landscape, trail amenities, and signage on mainline trails identified in this study. Optional spur connections are not included and should be identified separately during implementation.

Mileage	Trail Segment	P&E	Construction	Total
1.2	Segment 1   Clairmont to Fisher Trail Park	\$264,000	\$5,430,000	\$5,694,000
1.5	Segment 2   Fisher Trail Park to Shallowford Rd	\$277,000	\$6,081,000	\$6,358,000
1.4	ALT. shared-use option	\$269,000	\$5,329,000	\$5,598,000
2.1	Segment 3 - Shallowford Rd to Chamblee Tucker Rd	\$275,000	\$8,993,000	\$9,268,000
1.2	Segment 4 - Chamblee Tucker Rd to Presidential Pkwy	\$223,000	\$4,747,000	\$4,970,000
0.4	<i>Spaghetti Junction</i> Option A	\$432,000	\$2,079,000	\$2,511,000
0.4	<i>Spaghetti Junction</i> Option B	\$404,000	\$1,940,000	\$2,344,000
0.5	<i>Spaghetti Junction</i> Option C	\$443,000	\$4,240,000	\$4,683,000

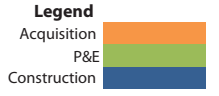
The total cost for implementation of the 8.3 to 8.5 miles of North Fork PCG ranges from \$37.2M to \$40.3M. Total cost for P&E is approximately \$1.7M. Total cost for construction varies from \$35.5M to \$38.5M. Construction cost per mile varies from \$4.2M to \$4.5M.



**Implementation Timeline & Fiscal Year Cash Flow Projection**

Below is a chart showing the timeline to implement the proposed North Fork PCG trails by 2030. The bottom of the chart outlines a summary of cash flow needed from all funding sources per fiscal year.

North Fork Peachtree Creek Greenway Study - Implmentation Timeline																																																																																																											
Implementation Phase, Task, and Costs per Task	DeKalb Co Fiscal Yr	# months	2022				2023				2024				2025				2026				2027				2028				2029				2030				2031																																																																				
			J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S
1	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$264,000	[Green bars]																																																																																																								
	Construction	\$5,430,000	[Blue bars]																																																																																																								
2	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$277,000	[Green bars]																																																																																																								
	Construction	\$6,081,000	[Blue bars]																																																																																																								
3	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$275,000	[Green bars]																																																																																																								
	Construction	\$8,993,000	[Blue bars]																																																																																																								
4	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$223,000	[Green bars]																																																																																																								
	Construction	\$4,747,000	[Blue bars]																																																																																																								
SJ (Box Culvert Option)	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$404,000	[Green bars]																																																																																																								
	Construction	\$1,940,000	[Blue bars]																																																																																																								
5	Acquisition	-	[Orange bars]																																																																																																								
	P&E	\$282,000	[Green bars]																																																																																																								
	Construction	\$9,040,000	[Blue bars]																																																																																																								
<b>Total per Fiscal Year</b>			\$198,000				\$2,404,200				\$4,585,000				\$3,841,100				\$4,895,900				\$5,618,800				\$4,972,600				\$5,682,400				\$4,206,000				\$1,552,000																																																																				





### *Funding Strategy*

It is recommended that every effort be made to identify funding locally for developing initial trail phases rather than relying on funding from Washington or Atlanta. The use of federal/state funding can be programmed several years in advance for extending the initial phases of the system. A public-private partnership should be created to fund development and expedite delivery of trail segments. Here are funding sources to consider when trails are built:

#### *Local Funding*

A variety of opportunities for local funding should be considered, including:

- Allocations within city/county budget
- Bond referendums
- Sales surtax generated fund
- Development impact fees
- In-kind products and services
- Philanthropic grants
- Transportation Special Purpose Local Options Sales Tax (TPLOST)

#### *State of Georgia Funding*

- Special project allocation
- Georgia Outdoor Stewardship Program (GOSP)

### *Federal Funding*

Federal grants and funding program opportunities for pedestrian and bicycle projects include:

- Metropolitan Planning Organization (MPO) prioritizes and distributes federal transportation funding through the Regional Transportation Investment Generating Economic Recovery Discretionary Grant Program (TIGER)
- Federal Transit Administration Capital Funds (FTA)
- Recreational Trails Program (RTP)
- Congestion Mitigation and Air Quality Improvement Program (CMAQ)
- Highway Safety Improvement Program (HSIP)
- Land and Water Conservation Fund (LWCF)
- National Highway Performance Program (NHPP)
- Outdoor Recreation Legacy Partnership Program (ORLPP)
- Surface Transportation Block Grant Program (STBG)
- Transportation Alternatives Set-Aside (formerly Transportation Alternatives Program) (TA)
- Transportation Improvement Program (TIP)



### Formation of an Implementation Committee

The Working Group, which helped guide the trail study process needs to evolve into a new Implementation Committee. Upon adoption of the N. Fork PCG Comprehensive Study, the committee will be tasked with overseeing and encouraging implementation of the trail system.

The Implementation Committee needs to be a politically savvy group that can raise public and private funding for the project. In addition, there needs to be adequate knowledge of the construction process among committee members to garner respect and confidence from the city/county and the citizens at large. The committee would benefit from individuals filling the following roles:

- Key Working Group members to ensure continuity
- City of Chamblee / Dekalb County Staff staff from appropriate departments
- Pro bono real estate or right-of-way specialist
- Pro bono attorney
- Fundraising Specialist/Foundation Representative
- Police/Fire representatives
- Design/construction team member

The Implementation Committee should assume oversight of the project as soon as the County adopts the final study.

### Next Steps

- Adoption by the Dekalb County Commission
- Establish Implementation Committee
- Allocate acquisition, P&E, and construction funding for Model Project(s)
- Complete P&E for Model Project(s)
- Acquire key parcels.
- Advance Model Project(s) to construction
- Review Implementation Priority plan and advance remaining segment(s) toward implementation.





04

# Design Standards



# Design Standards

## Overview

The existing Peachtree Creek Greenway trail segment in Brookhaven, GA has already become a staple trail for DeKalb County. The following section provides a summary of details and standards as used along the model mile. These standards will help create a uniform, regional trail system that future users will recognize as being unique to the greenway.

### **Trail Signage Standards**

### **Construction Standards**

- Greenway Trails
- Side Paths
- Trail Finish and Centerline Details
- Intersection Details
- At-grade Crossing Standards
- Structural Slab Crossing
- Tree Protection
- Bridges and Boardwalks
- Retaining Wall Finishes
- Trail Underpasses

### **Amenities and Design Elements**

- Pocket Parks
- Site Furnishings

### **Lighting and Security Measures**

- Trail Lights
- Security Cameras
- Fences and Handrails
- Private Trail Access

## Design Guidelines

The proposed trail segments should be designed and constructed in accordance with certain guidelines developed by various governmental agencies. All standards proposed for the N Fork PCG Trail Study are intended to meet or exceed the guidelines listed below:

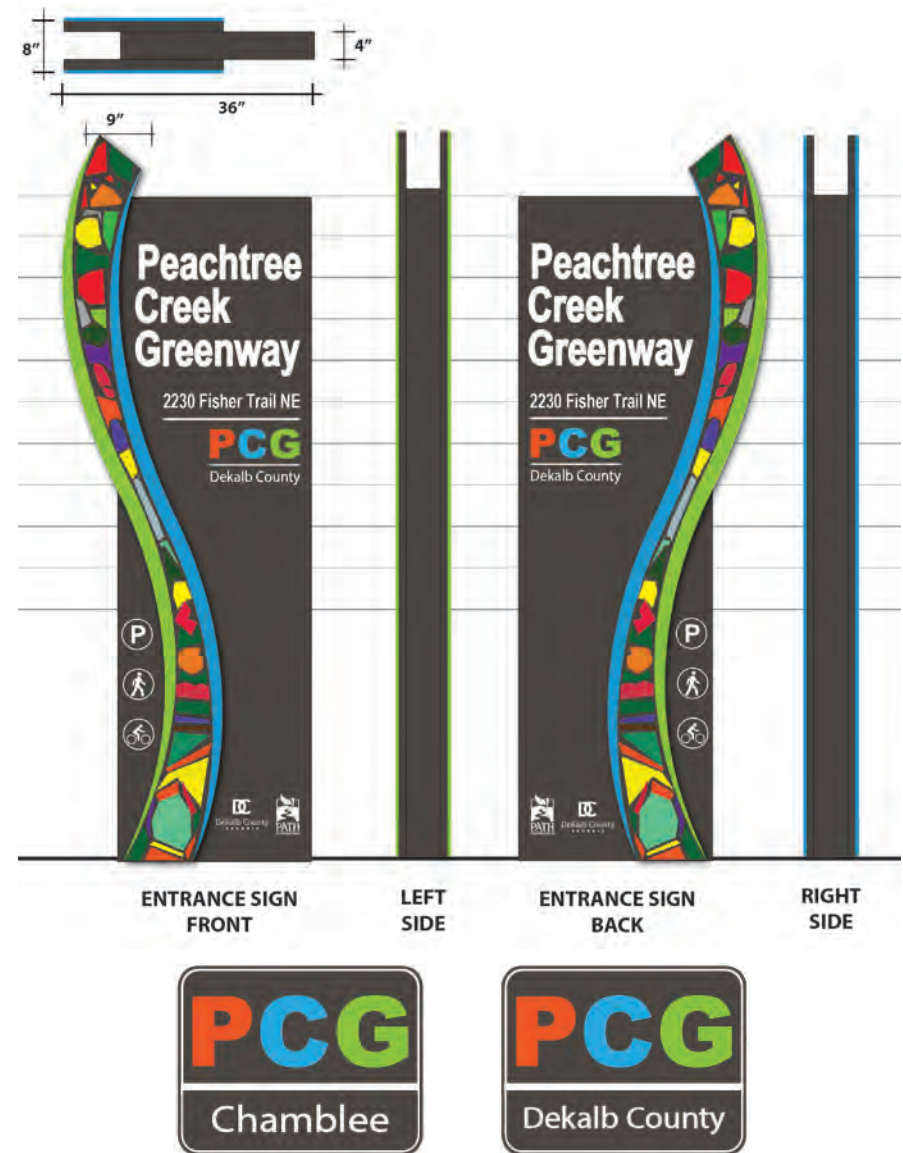
- 2010 Americans with Disabilities Act (ADA) Standards for Accessible Design
- Architectural Barriers Act (ABA) Accessibility Standards
- AASHTO Guide to Development of Bicycle Facilities, 2007
- MUTCD (Manual on Uniform Traffic Control Devices), 2009
- NACTO Urban Bikeway Design Guide, 2014



## Trail Signage Standards

The sign types for the N Fork PCG Trail Study are trailhead entrance signs, information kiosks, secondary directional signs, regulatory signs, and mile markers. The design style and the materials used in the sign structure represent the collective, diverse community that lives along the North Fork Peachtree creek. The following pages present the proposed trail signage that conveys the overall design intent.

- Trailhead Entrance Signs - these signs are to be placed at trailheads and/or parking lot entrances to help direct vehicles. The sign panel will have the mosaic band and the name Peachtree Creek Greenway internally lit.
- Kiosk Signs – these are information signs to be placed at trailheads along the N Fork PCG Trail Study. The sign panels will provide information on trail rules, trail etiquette, recognition/acknowledgment, and/or a trail map with distance information to regional destinations.
- Secondary Directional Signs – these are signs for identifying access points to the trail system from spur trails to neighborhoods, commercial areas, or shared use parking areas. The sign panels will contain the Peachtree Creek Greenway logo and provide directional information.
- Regulatory Signs – these are the most frequent signs along the greenway trail system. The sign panels will vary depending on information needed for the trail user to safely navigate the trail system.
- Mile Marker Signs – these are located at each one-mile distance along the trail and will have the Peachtree Creek Greenway logo. The sign panel will show the distance in miles and kilometers and will also include the elevation of the trail at that location.

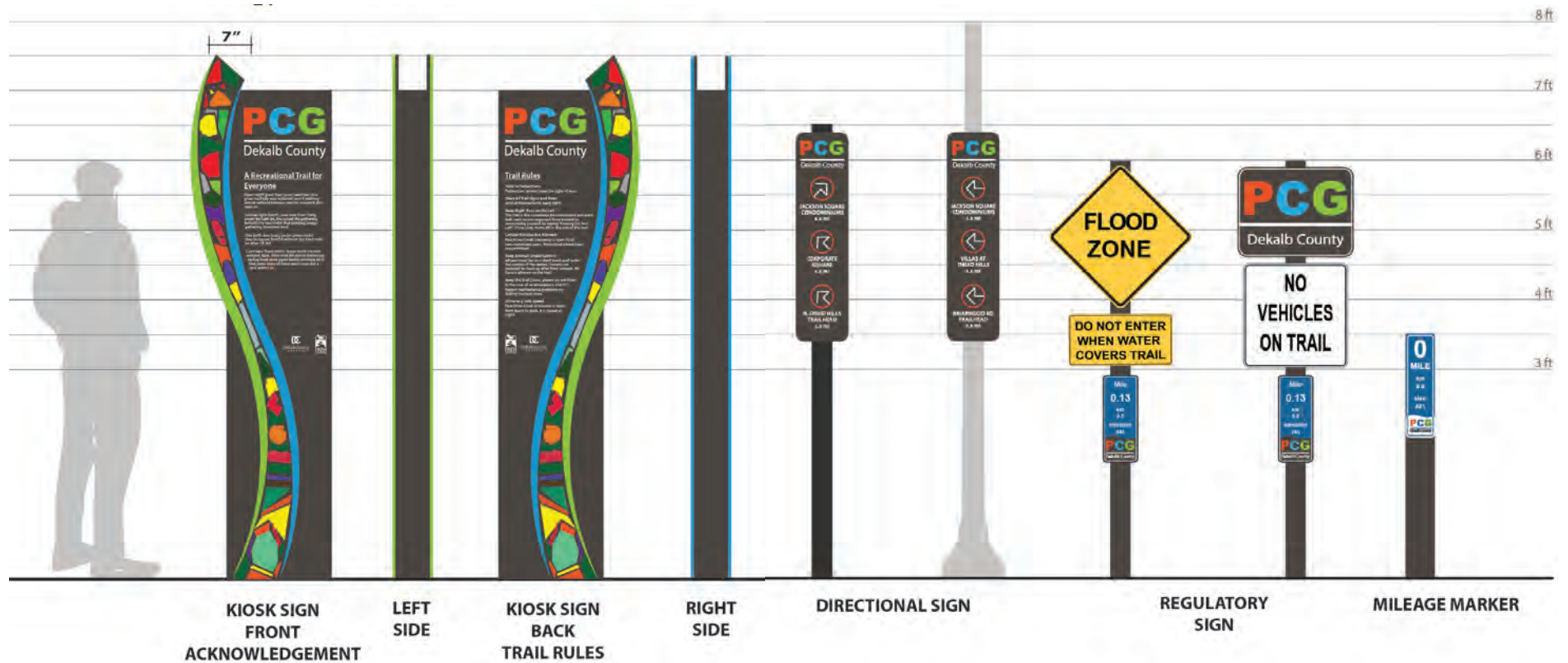


*All sign types incorporated along the greenway can be easily modified to reflect the location of any given trail segment.*



Trail Signage Standards (cont.)

The sign concepts presented below reflect the wayfinding typology established along the PCG in Brookhaven, GA. As the proposed greenway expands it's regional connectivity, the all signs can be modified to reflect the users location. (i.e. Dekalb County or City of Chamblee) All signs to be fabricated by aluminum panel with direct printed texts/graphics. 2-feet clearance needs to be provided from sign to the trail edge. Sign posts to be in-ground mounted into a concrete footer unless otherwise noted on construction details during implementation.





### Construction Standards

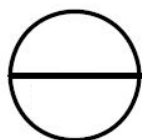
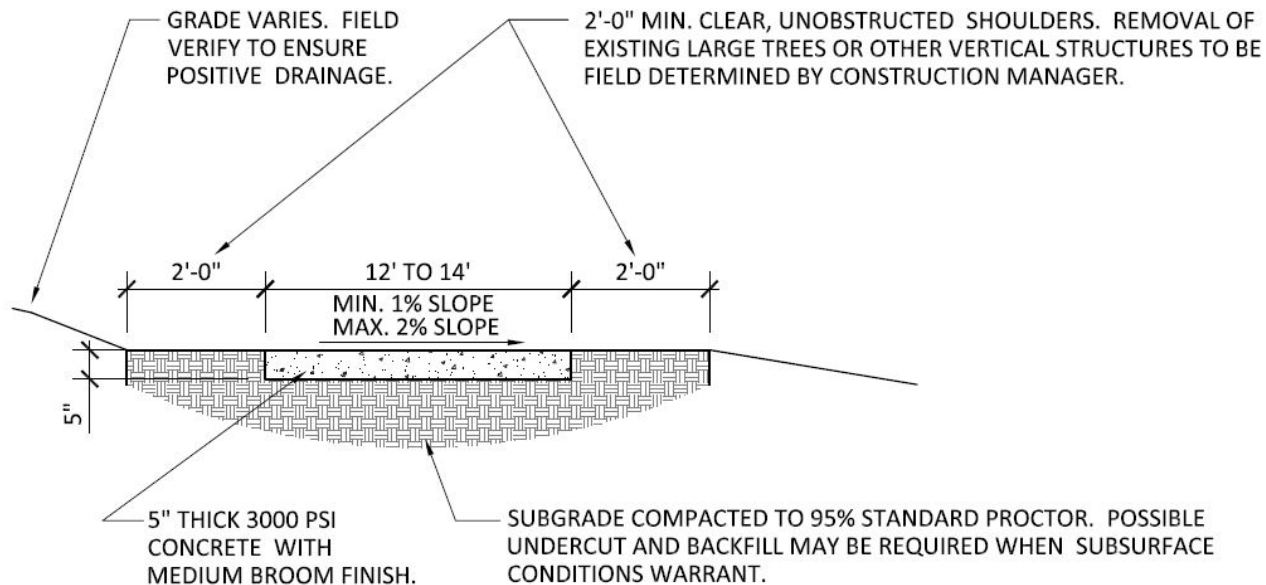
The drawings in the following section depict the typical sections for the proposed greenway trail and side path, with corresponding construction details and design standards.

#### Greenway Trails

Multi-use greenway trails with a 14-foot wide concrete surface provide for low long-term maintenance. All trails to have 4" x 12' sandblast centerline stripe and include stop-ahead markings when approaching an intersection.

##### NOTES:

1. 4" WIDE X 12' OR 14' ALTERNATING SANDBLAST CENTERLINE STRIPING TO BE INSTALLED ALONG ENTIRE LENGTH OF TRAIL. REFER TO 9/CD-01 FOR SANDBLAST CENTER LINE DETAIL.
2. CONTRACTOR TO SAW CUT CONTROL JOINT AT LEAST 1/4 DEPTH OF SLAB ACROSS ENTIRE WIDTH OF TRAIL. CONTROL JOINTS TO BE LOCATED THE SAME DISTANCE APART AS THE WIDTH OF TRAIL (I.E. 12' WIDE TRAIL TO HAVE CONTROL JOINTS EVERY 12' ALONG TRAIL). CONTRACTOR REQUIRED TO REMOVE SAW DUST AFTER CUTTING.
3. EXPANSION JOINTS TO BE LOCATED ALONG TRAIL MIN. EVERY 100' IN PLACE OF CONTROL JOINT.



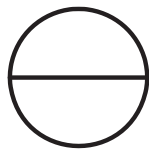
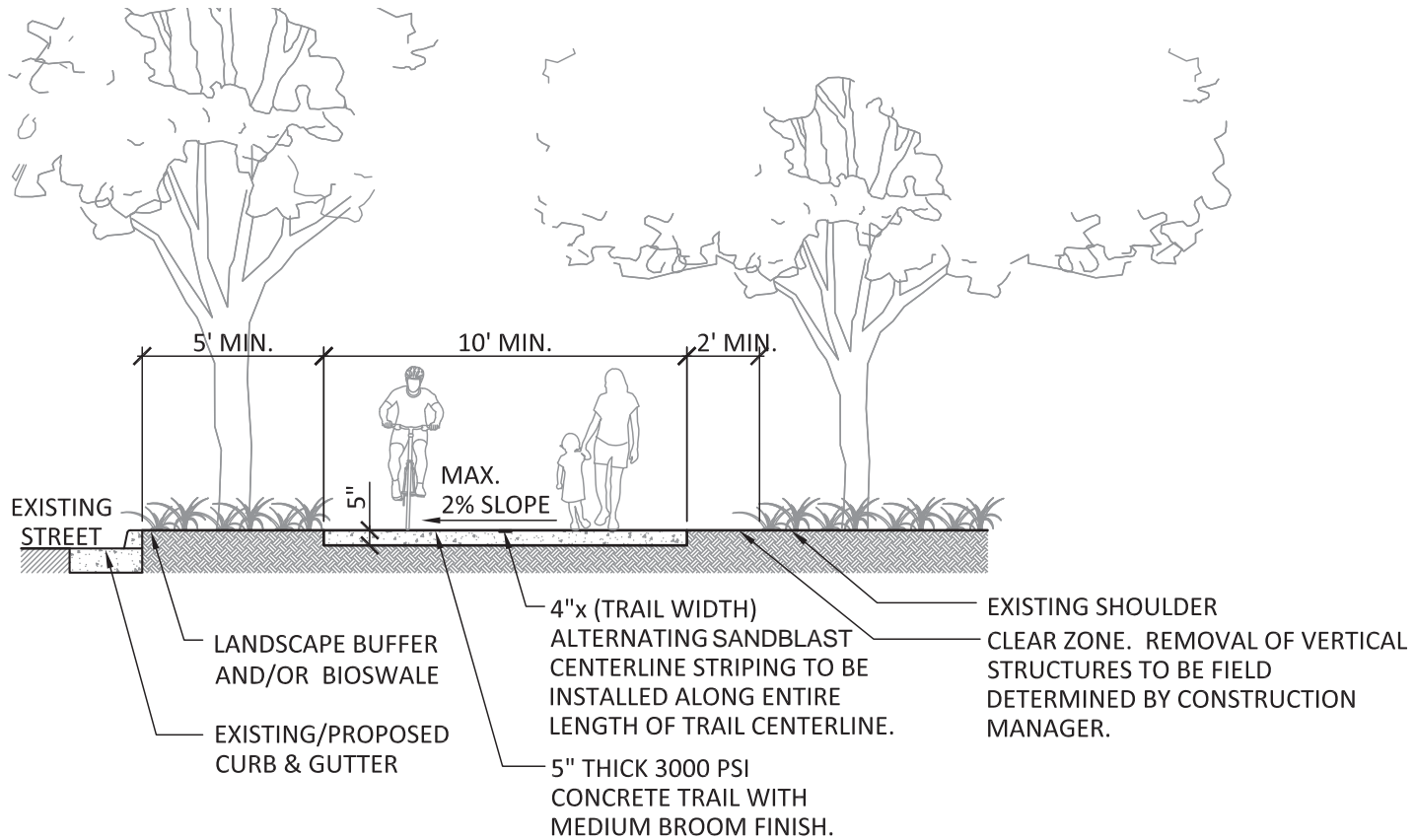
### TRAIL CROSS SECTION

SCALE: 1/2" = 1'-0"



## Side Paths

This typical cross section of a side path depicts a 10-foot trail with a 5-foot landscape buffer with 2' wide curb and gutter or 6" header curb.

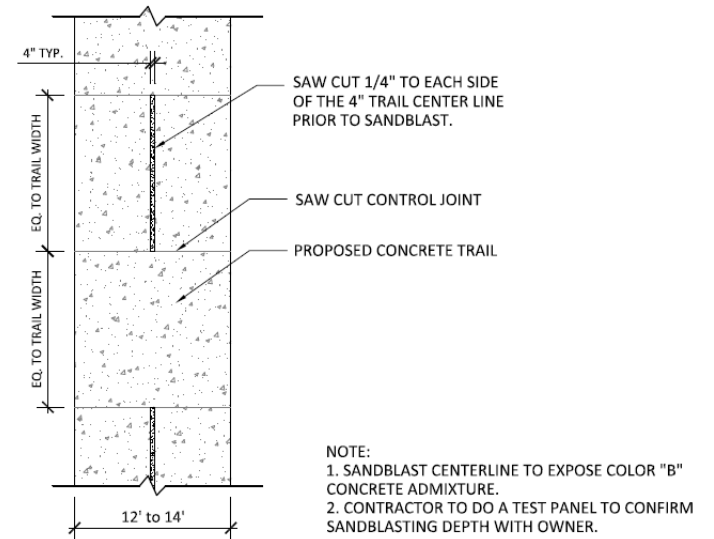


## SIDE PATH CROSS SECTION

SCALE: 1/4" = 1'-0"



### Trail Finish and Centerline Details



 **SANDBLAST TRAIL CENTER LINE DETAIL**  
 SCALE: 1/8" = 1'-0"

## Intersection Details

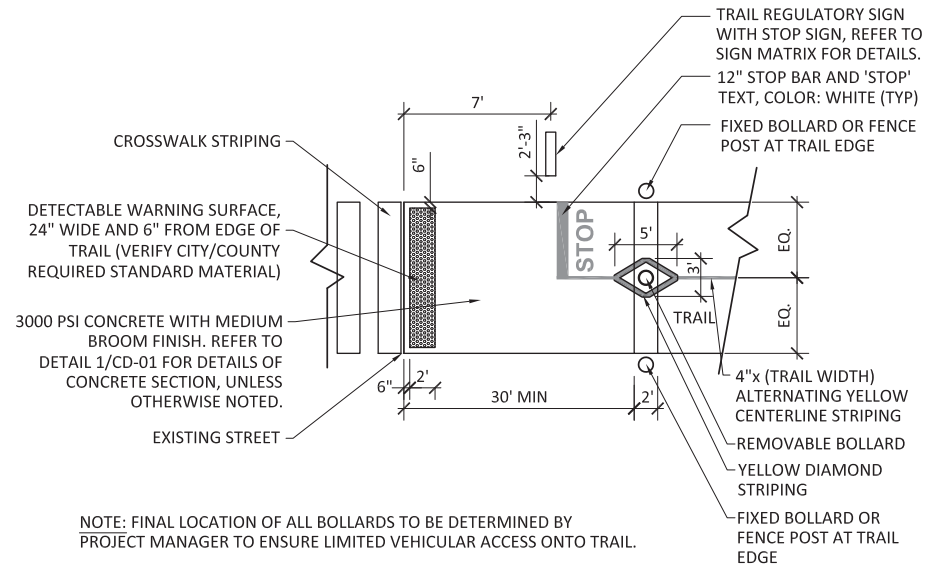
This page highlights typical trail intersection details and examples. Standard intersections include signage, bollards, and pavement striping.



An intersection example at Spanish Moss Trail, Beaufort, South Carolina

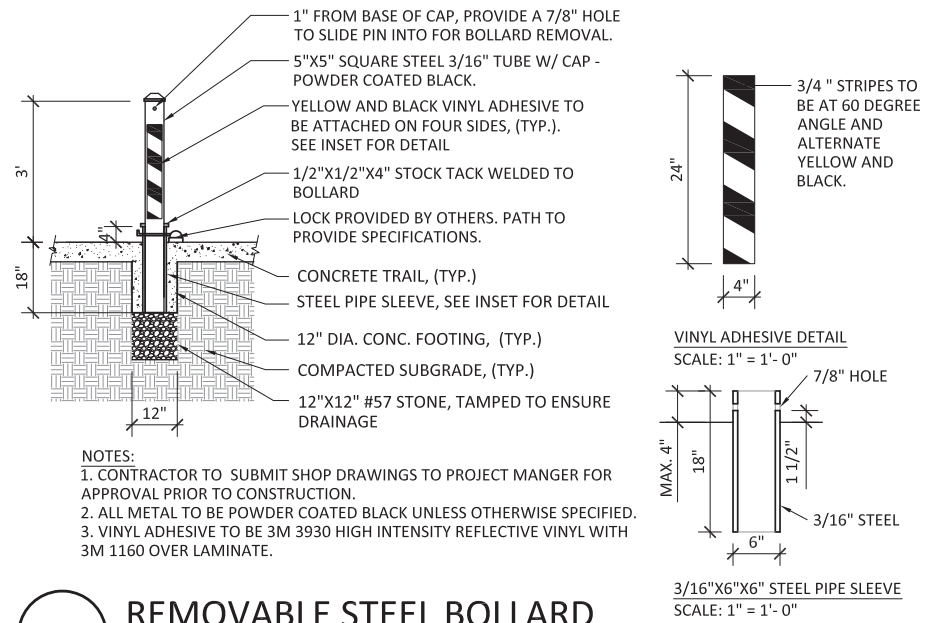


Typical trail intersection with signage, bollards, and pavement striping.



### STANDARD INTERSECTION: PLAN VIEW

SCALE: 1/8" = 1'-0"



### REMOVABLE STEEL BOLLARD

SCALE: 1/2" = 1'-0"



## At-grade Crossing Standards

U.S. Federal Highway Administration is promoting a series of pedestrian safety countermeasures through the Safe Transportation for Every Pedestrian (STEP) initiative. It outlines road diets, pedestrian hybrid beacons (PHBs), Pedestrian refuge islands, raised crosswalks, and crosswalk visibility enhancements as beneficial options for safe pedestrian crossing. These crossing, where applied will enhance the trail connection quality of the Peachtree Creek Greenway



*A combination of a long crossing distance and multiple lanes of oncoming traffic warranted the installation of a pedestrian refuge island.*

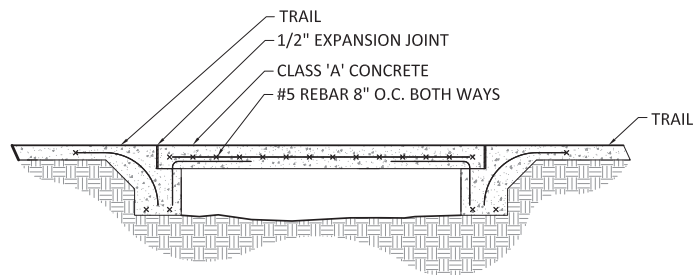


*A pedestrian Hybrid Beacon should be considered at locations where the trail needs to cross high speed roads or multiple lanes of traffic.*



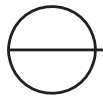
## Structural Slab Crossings

Structural slab is an environmental friendly solution for creating a drainage swale under the trail. This page shows the construction detail and examples of the structural slab crossing.



### NOTES:

1. CONTRACTOR TO PROVIDE SHOP DRAWINGS COMPLETED BY A LICENSED STRUCTURAL ENGINEER PRIOR TO CONSTRUCTION, FOR APPROVAL BY OWNER AND LANDSCAPE ARCHITECT.
2. TOTAL HEIGHT FROM TOP OF TRAIL SURFACE TO LOW POINT OF SWALE IS NOT TO EXCEED 30".



## STRUCTURAL SLAB CROSSING

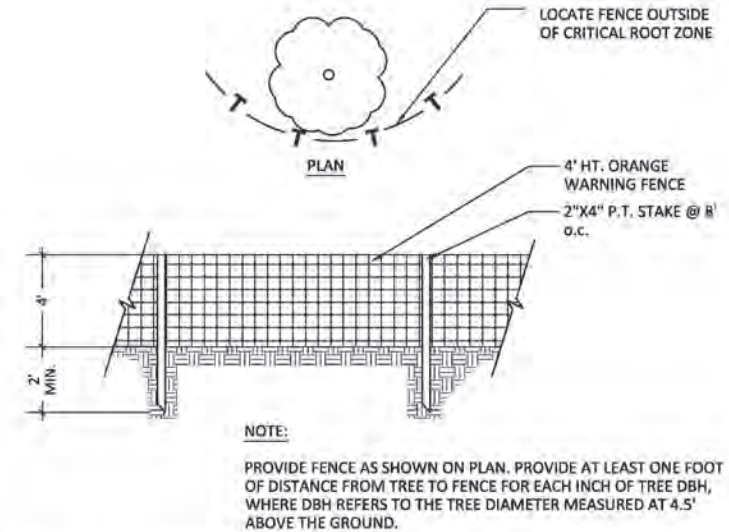
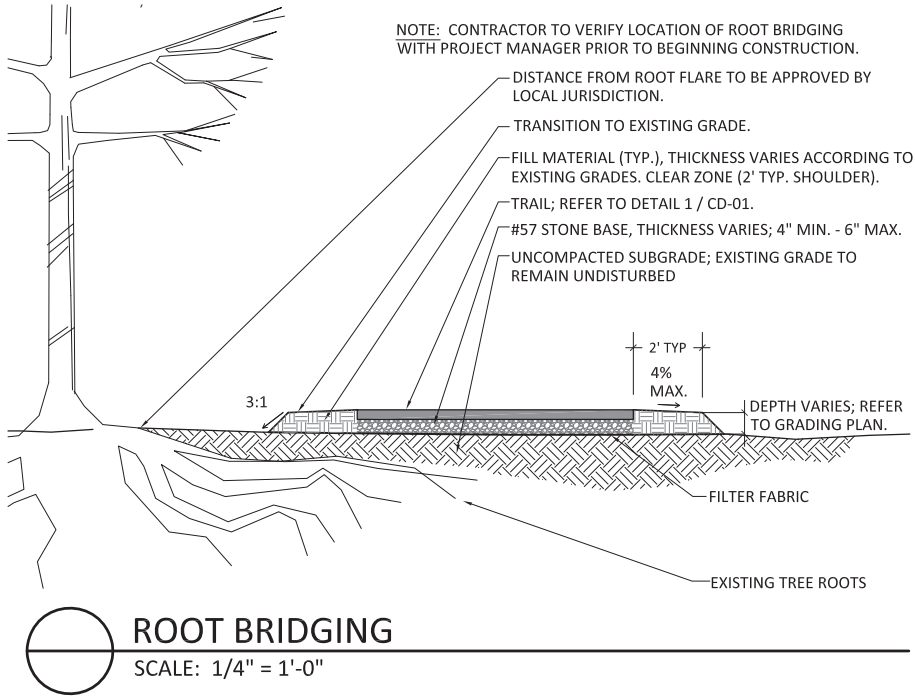
SCALE: 3/8" = 1'-0"





## Tree Protection

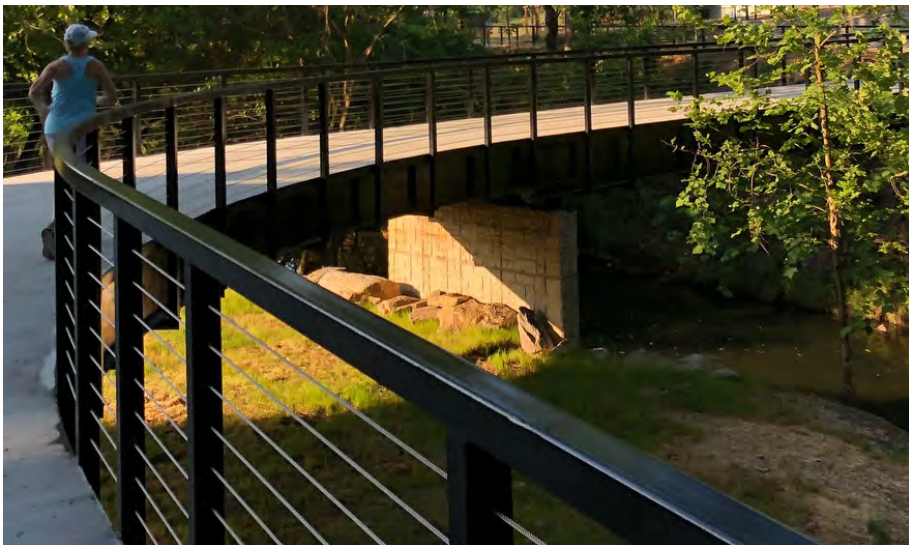
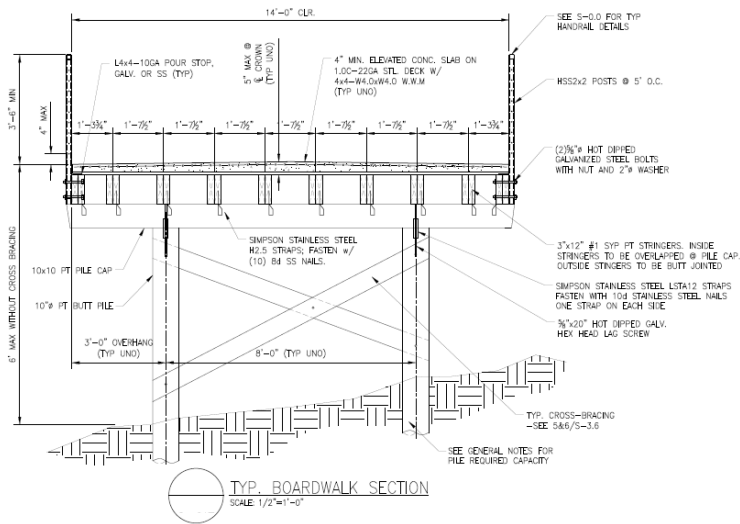
Root bridging ensures protection of trees and allows the trail to blend into a wooded setting. Tree protection fencing to be used as per Dekalb County standards and requirements.





## Bridges and Boardwalks

This page shows examples of a typical custom bridge, a prefabricated bridge, and a wooden boardwalk structure.



Bridges with painted finishes are timeless and suitable for urban environments

Top-down boardwalk construction uses wood pylons to minimize impact to the surrounding landscape. A concrete deck reduces enhances the trail's longevity by reducing the level of maintenance to the wooden structure.



Example of a prefabricated trail bridge over I-85 in Newnan, GA



## Retaining Wall Finishes

Retaining wall finishes can add texture walls of any height and add to the overall visual symmetry of the greenway.



*Example of Ashlar Finish along the Westside BeltLine Connector Trail*



*Example of Aloha Pattern wall finish along the PCG in Brookhaven, GA*

## Fences and Handrails

This page shows examples of post and cable fencing and handrail. The steel handrail post with galvanized cable can establish a semi-transparent look along the edge of trail.



*Shop painted metal post and cable handrail used along the PCG in Brookhaven, GA*



## Trail Underpasses

Box culverts offer opportunities for trail users to safely traverse major road barriers without having to interact or interfere with vehicular traffic.



*Multi-use trail going under a roadway adjacent to the North Fork Peachtree Creek.*



*Box culvert structure allows the trail to pass under a roadway where existing road bridge does not exist.*



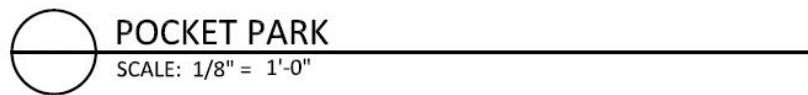
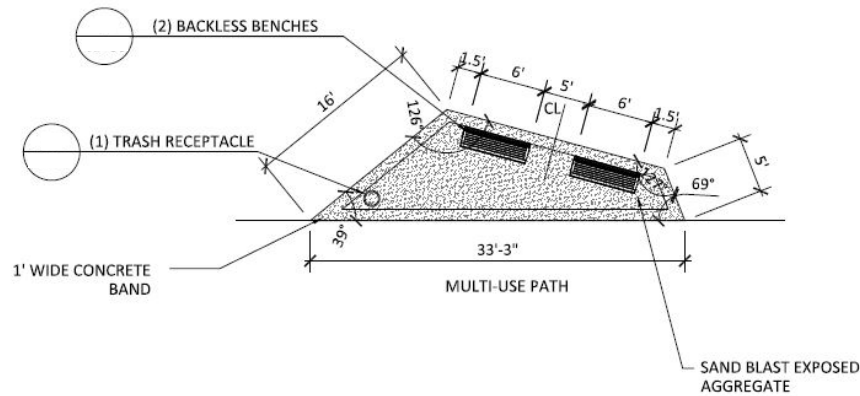
*Multi-use trail going through an existing creek box culvert under a roadway.*



## Amenity Standards

### Pocket Parks

Pocket parks are important amenities for the *Peachtree Creek Greenway* as they provide rest areas for trail users and a meet-up location for friends and neighbors. Pocket parks are to be located at all trail access points. Additional rest areas between destinations may be identified during design as well. Standard furnishing and signage will be placed at pocket park locations to provide seating and information about the trail system.



Example of existing, standard pocket park along the PCG in Brookhaven, GA

## Site Furnishings

As a complement to the trail signage, the trail amenities chosen for the *North Fork Peachtree Creek Greenway* are an extension of the amenities found along the model mile. The style and materials entail a traditional powder coated metal finish. This will allow for a visually uniform experience between the existing and proposed trails and help tie the *Peachtree Creek Greenway* together as one regionally connecting trail system.



### **Knight Bench by Forms+Surfaces**

model # SBKNI-072BA  
Description: 6-foot bench  
with back and arm rests,  
surface mounted

Bench frame powdercoat:  
Silver Texture  
Aluminum slats powdercoat:  
Slate Texture



### **Knight Bench by Forms+Surfaces**

model # SBKNI-072NA  
Description: 6-foot bench,  
backless, surface mounted

Bench frame powdercoat:  
Silver Texture  
Aluminum slats powdercoat:  
Slate Texture



### **Standard Waste and Recycling Receptacle by Forms+Surfaces**

model # SLDIS-136  
Description: 36 Gallon, no concrete  
base

Lid color: Silver Texture  
Lid graphics: Litter/Recycling  
Body color: Slate Texture



## Site Furnishings (Cont.)



### Deluxe Single Pull Dog Station by Jazzy

Description: single pull station holds up to 400 bags, Commercial-grade aluminum, durable powder coated/UV protected finish in black



### Fixit Service Station by Dero

Description: Powder coated black; includes all tools necessary to perform basic bike repairs and maintenance with air pump kit 3.



### Bike Rack by Forms & Surfaces

model # SKCAP  
Description: solid cast aluminum, surface mounted

Powdercoat Color:  
Slate Texture

## Lighting and Security Measures

Trail lighting and security measures are a must for the *North Fork Peachtree Creek Greenway* in order to create a sense of safety and comfort for trail users any time of day.

### Trail Lights



MSA MESA LED

by Decorative Luminaire  
1-6 LightBARs  
Solid State LED

### Security Cameras



Example of pole mounted security camera used along PCG in Brookhaven, GA. Security cameras to be installed by Georgia Power Company



## Private Trail Access

The proposed N Fork PCG interacts with a multitude of multi-family housing properties. This fencing provides coded access points where only local users who know the code may freely move on and off the greenway



*Example of metal security fence and gate for secure access for residents living along the greenway*



*Example of coded access gate for multi-family housing along the PCG in Brookhaven, GA*

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