



South Peachtree Creek Trail

Northern Study Area

Date: 08/30/2022
Prepared for: DeKalb County, GA
Prepared by: PATH Foundation



DeKalb County
G E O R G I A



alta

KAIZEN
COLLABORATIVE

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South Peachtree Creek Trail Northern Study Area

Prepared for:



DeKalb County
GEORGIA

Prepared by:



August 30, 2022

Acknowledgment

SPCT Connectivity Study Northern Study Area - Working Group

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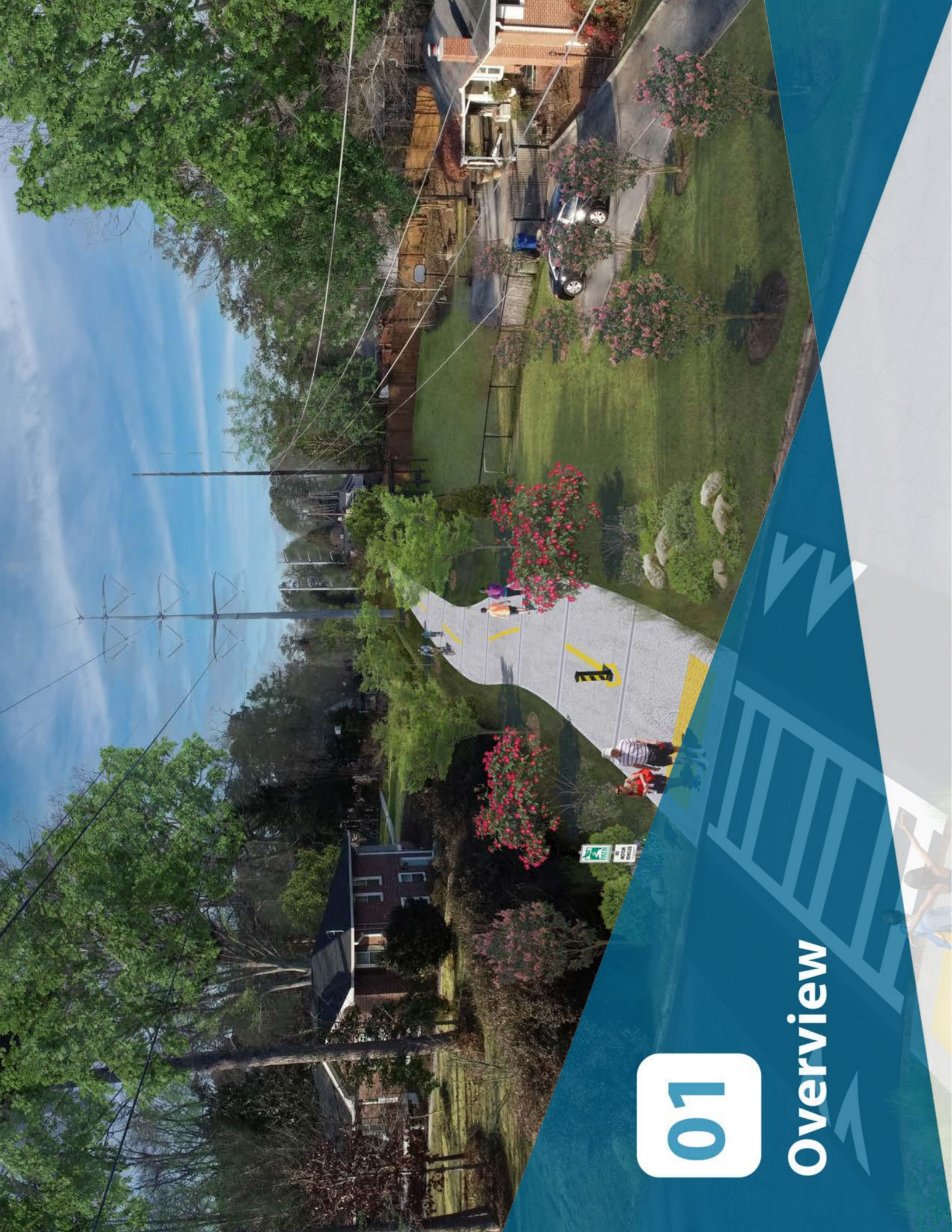
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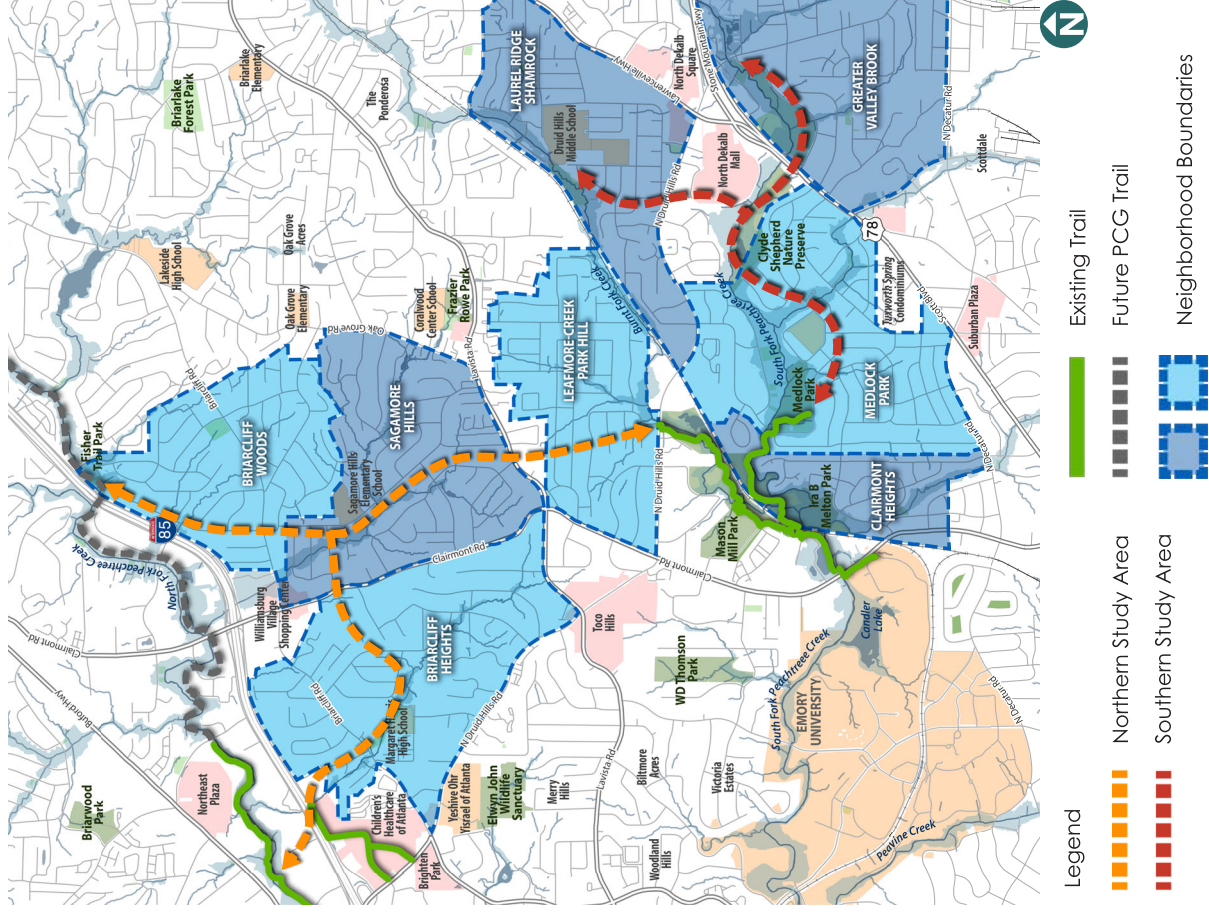
Overview

01 Overview

Project Background

DeKalb County has partnered with the PATH Foundation to continue promoting dedicated bike and pedestrian facilities throughout North DeKalb. The South Peachtree Creek Trail connectivity study seeks to compound the desires of residents, county officials, and businesses to find opportunities for multi-modal transportation in a vehicle-dominated area.

This study has been divided into two sections: the Northern Study Area and the Southern Study Area (see map to the right). This report focuses on the Northern Study Area, which aims to extend the reach of the South Peachtree Creek Trail at Mason Mill Park to the Peachtree Creek Greenway (PCG) in Brookhaven enabling residents and users to connect to DeKalb County schools, private schools, shopping areas, restaurants, Children's Health Care of Atlanta (CHOA), the Atlanta VA Medical Center and Emory University, as well as other destinations in the surrounding neighborhoods. Through the planning efforts of this study, the PATH Team identified opportunities for a regional trail system that will allow users to safely cross major vehicular arteries and offer alternative connections to major commercial hubs, schools, and existing parks.



Working Group

PATH established a working group to guide the SPCT Northern Study. The formation of the working group includes members from the DeKalb County Department of Transportation and DeKalb County Commissioners and Representatives.

Four meetings were held between the PATH Team and the working group via zoom during the six-month collaborative process. The working group collaborated with the PATH team to establish the overall goals for the study, provide feedback on preliminary findings, aid in public outreach, and review planning decisions. The updated findings of this study were circulated to collect feedback from the working group at the end of June 2022, prior to the creation of this document.

Study Goal

The goals of the South Peachtree Creek Trail (SPCT) Northern Area Study include:

- Determine a feasible mainline trail route to connect existing trails.
- Outline creative, safe, short-term solutions using neighborhood streets to connect to the mainline trail route.
- Engage the neighborhoods and stakeholders in the project study area to ensure equitable connections are being made.
- Provide the County with a priority list of projects for implementation.

Data Collection and Field Work

Using the DeKalb County GIS data, traffic count data at key locations within the study area, assessment of current and future planning and development efforts, as well as feedback from the working group, PATH's analysis of existing and proposed trail connections through DeKalb County focused on the following criteria:

- Feasible for construction
- Appeal to a broad group of users

- Safety of cyclists and pedestrians
- Connections to desirable destinations
- Connections to existing trails
- Grades and other user experience factors

While in the field, the potential connectivity corridors were examined to understand potential opportunities and challenges. Fieldwork allowed for additional opportunities to be identified, such as spur trails, access points, alternate routes.

Community Engagement

The South Peachtree Creek Trail Connectivity Study incorporated the input from area stakeholders and the general public through two community engagement meetings.

The first meeting was formatted as a public open house as the kick off of the study process. Team members interviewed over 100 attendees with map and image boards, gathering public input such as where does the attendee live or work in the study area, where are the desired destinations, where does the attendee see as opportunities for pedestrian/bicycle connectivity versus their concerns.

An online survey was distributed before and after the meeting to collect additional questions and feedback. The results showed an overall positive and supportive theme on improving a safe connective trail system to benefit the whole community, the idea of trail expansions, great work done by the County and the PATH Foundation. General concerns in the area were on crime, trash disposal, preservation of natural areas, aesthetics, viewshed, landscaping, and parking for trail users.

Community Engagement (continued)

After the first community open house event, the PATH team conducted additional fieldwork, and met with the working group to vet preliminary opportunities and challenges within the study area. The preliminary findings were then presented to the public at the second engagement meeting, which allowed both virtual and in-person attendance. The meeting included a Q&A session, another open house discussion with mapping and image boards, and comment cards. The presentation was published on the county's website with an extended period for comments.



Public Feedback Summary

Neighborhoods	Affirmations	Concerns	Responses
Leafmore	<ul style="list-style-type: none"> Supports for connectivity through Leafmore and Sagamore to I-85 corridor. 	<ul style="list-style-type: none"> Steep topography along utility corridor Utility corridor owned by abutting residential owners. The overall feedback was not supportive for a greenway trail running through the corridor. 	<ul style="list-style-type: none"> Due to challenging acquisition and coordination with Georgia Power, this study mostly identifies greenway connections outside of the utility corridor. Because there is limited opportunity for a safe and inviting connection in the center of Leafmore neighborhood, it is recommended to utilize the Georgia Power utility corridor between Leafmore Drive and Lavista Road.
Sagamore Hills	<ul style="list-style-type: none"> Improving connectivity and bike/ped safety through the neighborhood to future/existing trail networks Support of off-road trail at Council Bluff Support of traffic calming measures on parallel roads off the utility corridor 	<ul style="list-style-type: none"> Owners along the utility corridor were not supportive of an off-road trail Traffic safety on busy roads 	<ul style="list-style-type: none"> Calm street measures are recommended along identified neighborhood streets to support a safe bike/ped connection using existing right-of-way. This study also identified critical opportunities for installing safe crossing over Lavista Road and using the utility corridor abutting a multi-family residential property. Key acquisitions should be coordinated prior to implementation of this segment and the proposed crossing.
Briarcliff Heights	<ul style="list-style-type: none"> Support of off-road trail at Cliff Valley County-owned and multi-family residential properties along utility corridor could provide future greenway opportunities that entails less challenging acquisition. 	<ul style="list-style-type: none"> Some property owners on Audubon Drive were opposed for the cul-de-sac becoming a main bike/ped route 	<ul style="list-style-type: none"> The GDOT proposed project along S.R. 155 (Clairmont Road) presents an opportunity to provide a safe crossing across Clairmont Road. This segment is timely as the feasible options for connecting Briarcliff Heights to Sagamore Hills utilizes GDOT's proposed project. The completion of CHOA on North Druid Hills Road will necessitate a near-term multi-use trail to accommodate an increased number of commuters.

Types of Connectivity

Three types of trail facilities were identified from a multi-use trail perspective during the planning process to ensure the *South Peachtree Creek Trail* met the goals of being an inclusive, comfortable, implementable, inspiring, interconnected, and engaging network. Off-street connections are the most desired types as they offer the safest and highest quality connections for trail users. Where off-street connections are not feasible, this study suggests on-street connections.

Off-street Connection: Greenway Trail

Greenway trails are trails used by all non-motorized travelers and are constructed in green areas such as parks, stream corridors, undeveloped land, etc. Greenway trails should be a minimum of 10 feet wide, hard surfaced, with design and construction specifications following the American Association of State Highway Transportation Officials (AASHTO) regulations.



Greenway Trail Typical Section



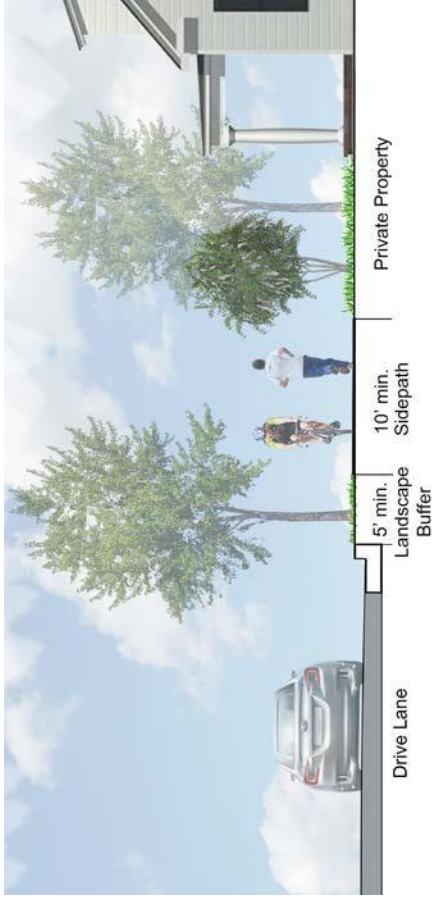
Greenway Trail Example: South Peachtree Creek Trail under Clairmont Road



Greenway Trail Example: South Peachtree Creek Trail at Mason Mill Park

Off-street Connection: Buffered Sidepath

Shared-use trails alongside roads in public right-of-way, or buffered side paths, offer an additional opportunity for making connections. Buffered side paths should be a 12 foot wide (or a minimum of 10 feet where 12 feet cannot be achieved). A successful buffered side path should be clear of all vertical elements such as signs, fire hydrants, and water valves. Side paths should have a 5-foot minimum landscaped buffer from the roadway per AASHTO requirements and on-street markings to alert drivers of the presence of bicycles and pedestrians.



Side Path Typical Section: Residential Street



Buffered Sidepath Example: Chastain Park in Atlanta, GA

On-Street Connection: Calm Street

A Calm Street refers to the use of traffic calming to slow down cars as they move through commercial and residential neighborhoods. This approach, when used on low volume, low speed roads, creates more of an equilibrium among all street users like cars, pedestrians, and bicyclists. Calm Streets are proposed when more defined facilities are not feasible to make critical connections on the trail system. Traffic calming elements that are used to achieve Calm Streets are:

- Directional signage
- Pavement markings
- Speed Cushions
- Mini Roundabouts
- Median Islands
- Chicane Street
- Intersection Upgrades
- Sharrow markings and signage

Benefits of Calm Streets include:

- Promotes slower and more careful driving which leads to fewer and less severe crashes
- Calm Streets are more equitable
- Encourages people of all ages and abilities to feel safe using their neighborhood streets
- Traffic calming elements can beautify the neighborhood or create a sense of place



Calm Street example showing Median Islands as traffic calming elements.



Calm Street example showing a Mini Roundabout as a traffic calming elements.



02

SPCT Northern Study Area

02 Northern Study Area Preliminary Findings

Overview

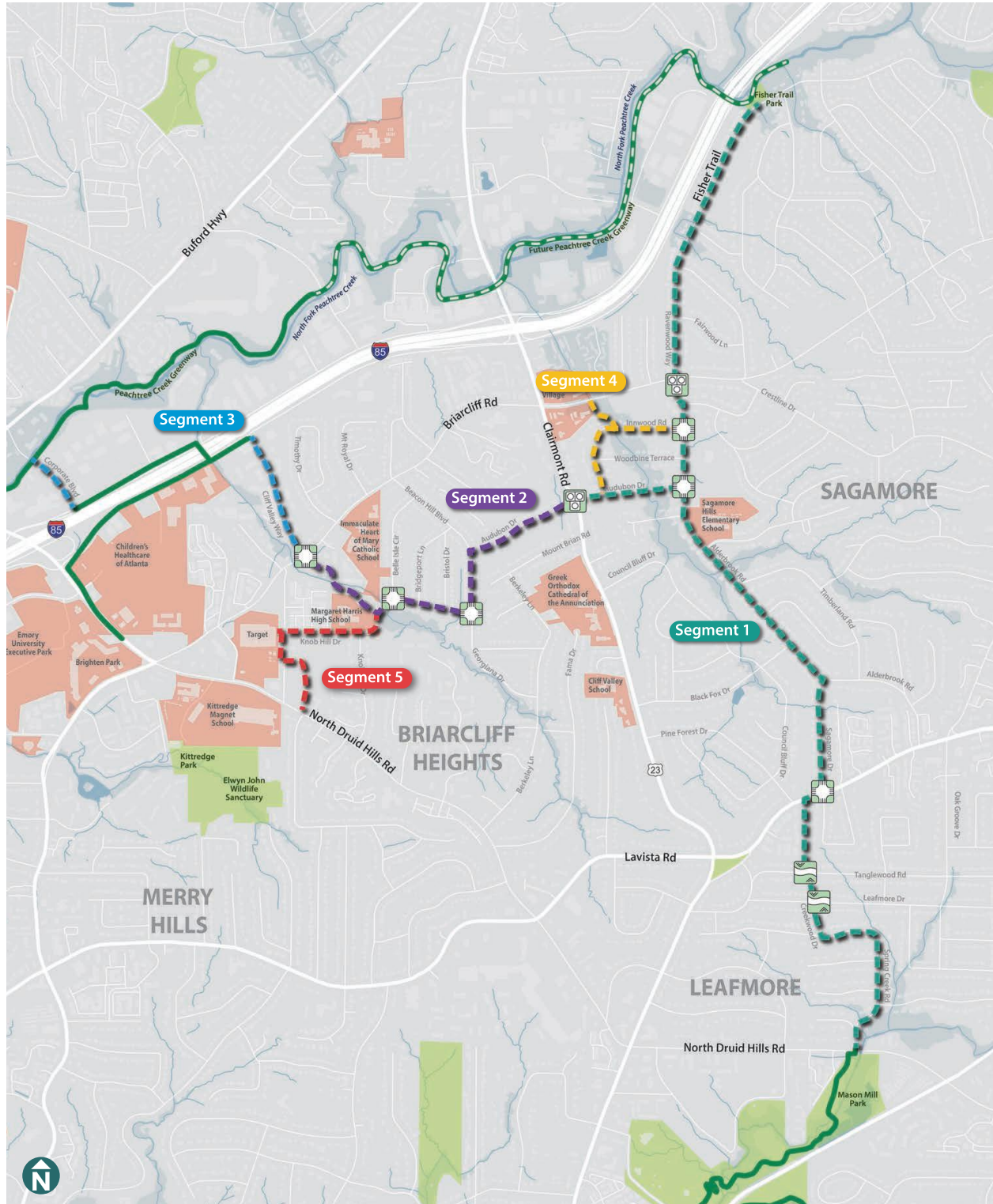
Based on the study goal established by the working group and extensive field research, the PATH Team has identified 1.30 miles of multi-use greenway and buffered side-path trail and 6.1 miles neighborhood connectivity opportunities that will help expand regional connectivity through DeKalb County and provide a safe and engaging trail network to future users.

The development of the South Peachtree Creek Trail Northern Study Area offers DeKalb County residents access to a multitude of business centers, schools, restaurants, and communities without the use of a car. The complete trail system will safely connect the many communities along the South Fork Peachtree Creek to parks, transit, local businesses, attractions, and regional connectivity networks such as Peachtree Creek Greenway, Atlanta Beltline, and the planned North Fork Peachtree Creek Greenway, and Gwinnett County trail system.

Within the overall study, the proposed route is divided in five (5) segments as shown to the right. The adjacent map shows the beginning and end of each segment. The following pages will present each trail segment in more detail, including opportunities and potential challenges that will likely affect decisions regarding the priority of implementation. A detailed map for each segment is included with existing images, as well as before and after vision graphics of selected locations along each proposed trail segment.

Trail Segments

- 1 | N. Druid Hills Drive to Fisher Trail Park
- 2 | Briarcliff Heights Connector
- 3 | Cliff Valley Way to Peachtree Creek Greenway
- 4 | Williamsburg Village Connector
- 5 | Knob Hill Drive Connector



Legend

- Study Segments
- Existing Trail
- - - Future Trail
- Park
- Floodplain
- Point of Interest
- Intersection Improvements
- Signalized Crossing
- Trail Crossing
- Roundabout Improvements

Segment #1 | N. Druid Hills Road to Fisher Trail Park

Description

Segment 1 begins as a neighborhood greenway that stems from the existing South Peachtree Creek Trail at the intersection of North Druid Hills Road and Spring Creek Road. The neighborhood greenway follows Spring Creek Road north to Creek Wood Drive north where it will cross Leafmore Drive and enter the Georgia Power utility corridor. Using the utility corridor requires minimal acquisition from property owners. The trail continues north through the corridor until it crosses Lavista Road (SR236) with a pedestrian-activated street crossing device. Pedestrians can then use existing sidewalk leading east. Once at Sagamore Drive, calm street treatments will allow trails users to travel along Sagamore Drive, Alderbrook Road, Segment 1 creates a safe connection from neighborhoods to schools, places of employment, restaurants, parks, and existing trails.

Overview

Distance: 16,899 Linear Feet (3.2 miles)

Begins: North Druid Hills at Spring Creek Road

Ends: Fisher Trail Park along Fisher Trail

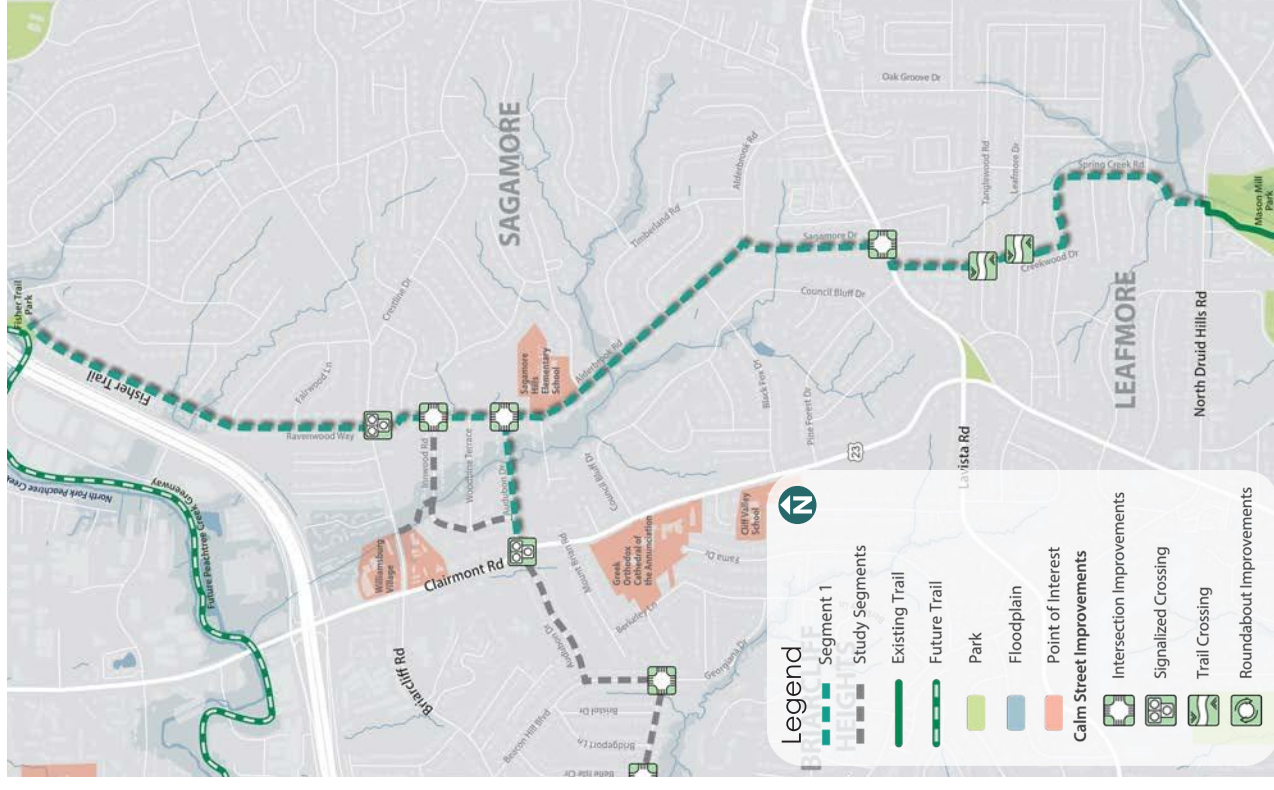
Connecting Destinations: South Peachtree Creek Trail at Mason Mill Park, Fisher Trail Park, Leafmore and Sagamore Hills Neighborhoods, Sagamore Hills Elementary School, the future Peachtree Creek Greenway, Atlanta Beltline and Gwinnett County trail system.

Opportunities and Benefits

- Establishes a short-term bike/ped connectivity through the Sagamore and Leafmore neighborhoods via Calm Streets treatment within public right-of-way.
- Provides a safe crossing of Lavista Road
- Achieves the connectivity goal of the project by connecting to the existing South Peachtree Creek Trail and future North PCG corridor at Fisher Trail Park
- Sets the stage for future connectivity and the crossing of Clairmont Road

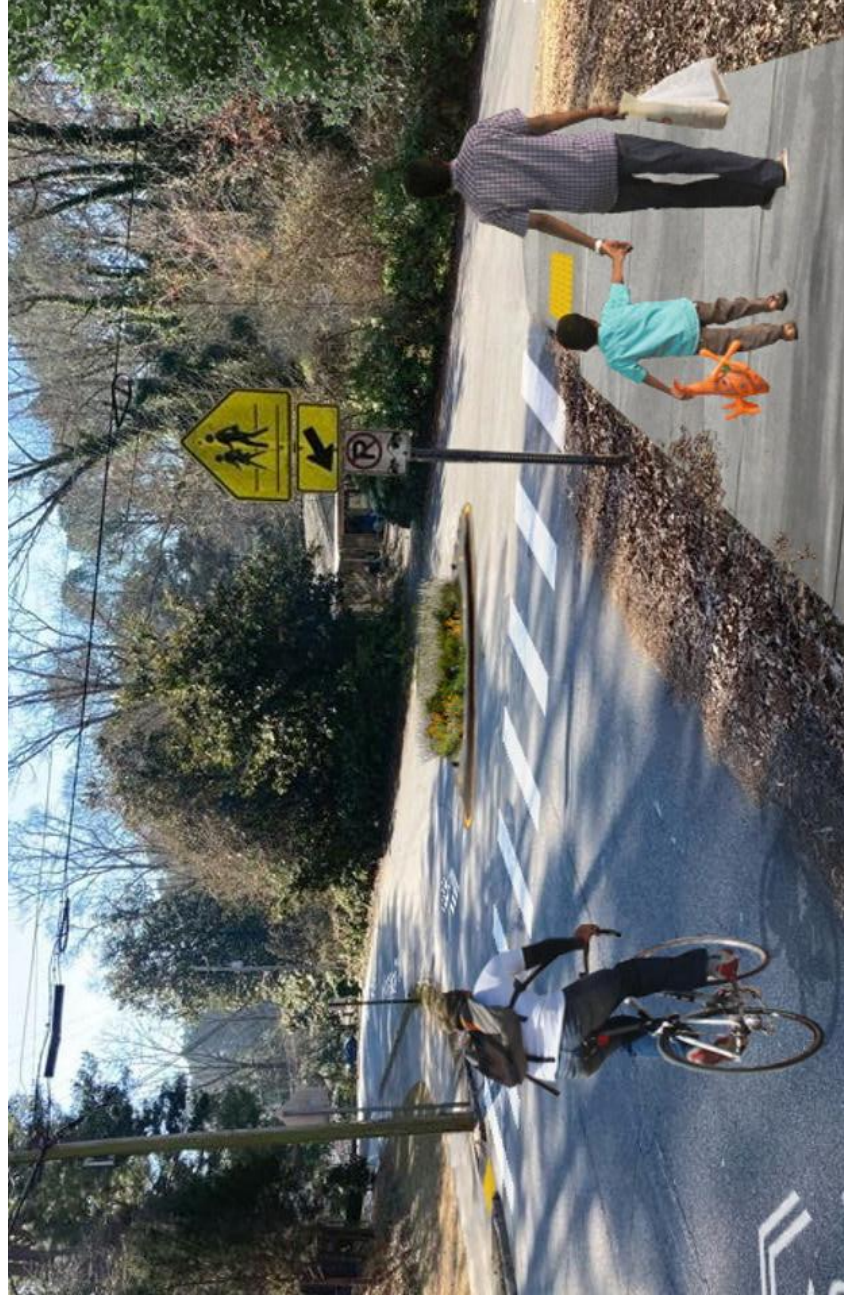
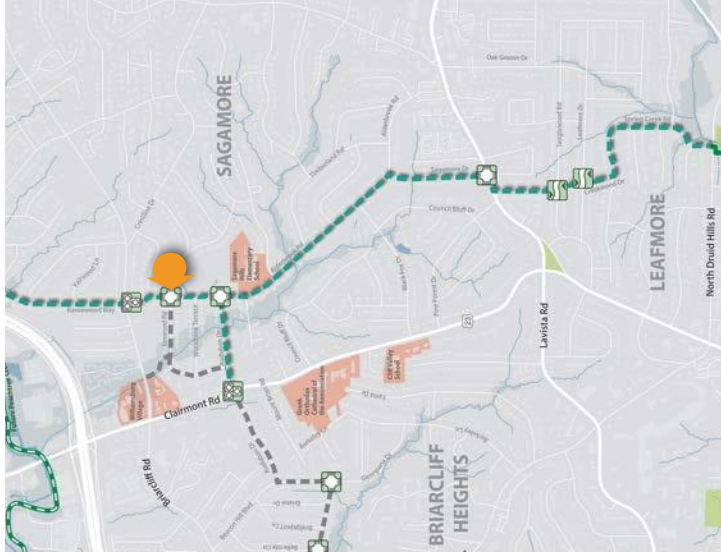
Potential Challenges

- Requires GDOT coordination on Lavista Road (SR 236) and Audubon Drive with incorporating a Pedestrian Hybrid Beacon at Clairmont Road
- Easements are required from critical private property owners
- Traffic signal modifications are required at multiple intersections
- Steep Terrain



Segment #1 | Proposed Calm Street Tools

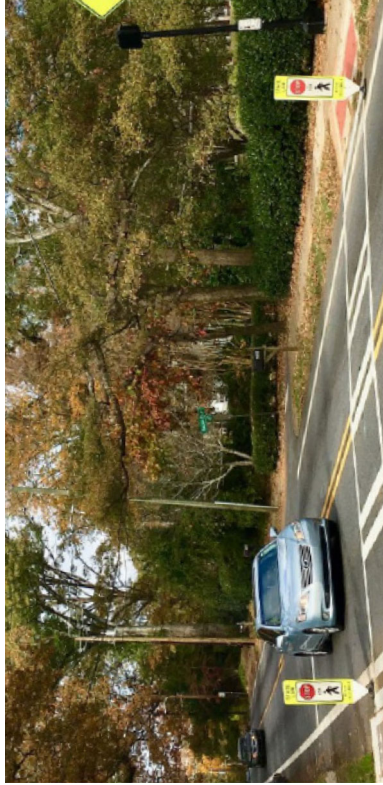
At this intersection of Ravenwood Way and Alderbrook Road, traffic calming elements like pavement marking and a median island are being proposed to make this crossing safer for pedestrians and cyclists. The pavement marking will help make people in vehicles more aware of bikers' presence and the median island creates a buffer between traffic flows and a visual reminder to slow down at this turn.



Existing conditions at the intersection of Ravenwood Way and Alderbrook Road facing East.

Segment #1 | Proposed Calm Street Tools

Below are other treatments being proposed for Segment 1 to help calm traffic while providing safety measures as well as wayfinding information for users.



Refreshed crosswalk striping for better visibility and safety.



Median refuge islands at intersections for safe crossing.



Bicycle markings to indicate the roads are shared along this network.



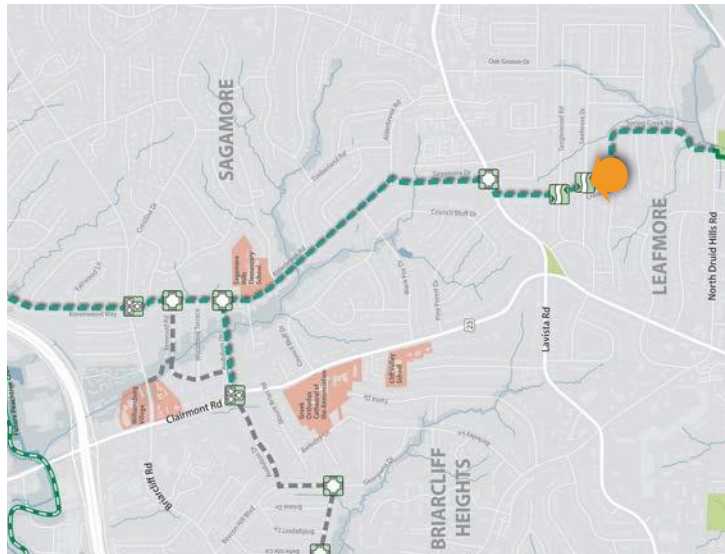
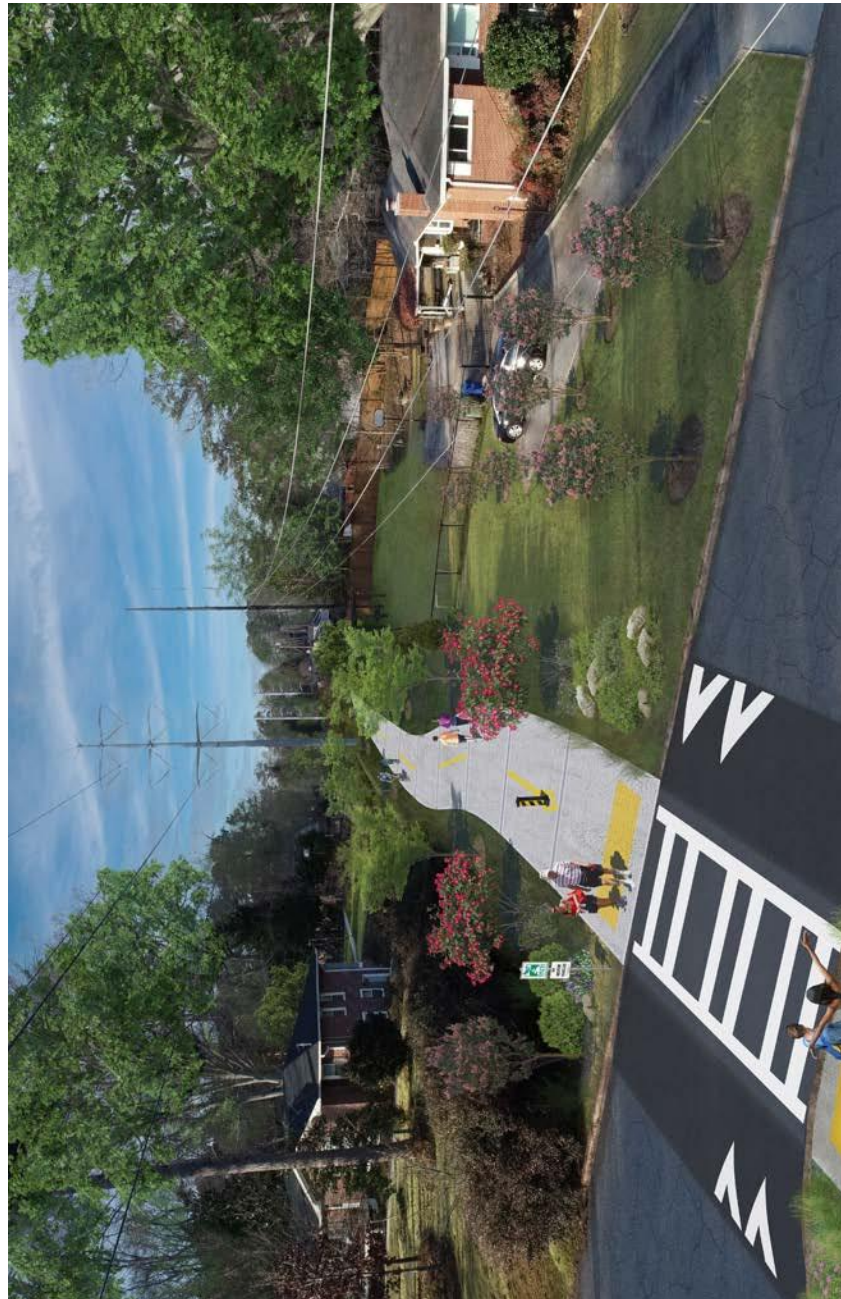
Example for wayfinding pavement markings.



New and upgraded speed cushions.

Segment #1 | Proposed Greenway Trail

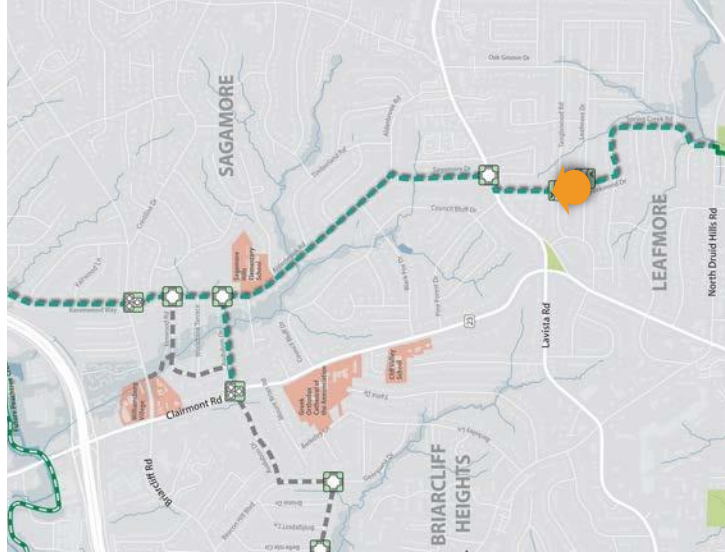
The vision graphic below depicts a 12' wide proposed trail going through a power line corridor. This image shows a pedestrian crossing to accommodate trail users to cross Leafmore Drive in an easy and safe way. The pedestrian crossing also acts as a traffic calming element on the residential road Leafmore Drive.



Existing conditions at the intersection of Creekwood Drive and Leafmore Drive facing North through the power line corridor.

Segment #1 | Proposed Greenway Trail

The vision graphic below depicts a 12' wide proposed trail going through a power line corridor. This image shows a pedestrian crossing to accommodate trail users to cross Tanglewood Drive in an easy and safe way. The pedestrian crossing also acts as a traffic calming element on the residential road Tanglewood Drive. This greenway trail is to continue through this corridor until it reaches Lavista Road.



Existing conditions at the intersection of Creekwood Drive and Leafmore Drive facing North through the power line corridor.



Segment #2 | Briarcliff Heights Connector

Description

Segment 2 begins at the intersection of Audubon Drive and Clairmont Road. The Georgia Department of Transportation (GDOT) is designing a safe pedestrian crossing at this intersection. Segment 2 would utilize this crossing and continue the route west as a neighborhood greenway to the dead end of Audubon Drive. At this dead end, a greenway trail would be built to connect to Beacon Hill Boulevard. At Beacon Hill Boulevard, the route becomes a neighborhood greenway heading south until it reaches Belle Isle Circle and leads west where it will again turn south onto Knob Hill Road. Once at the greenspace north of Margaret Harris High School, the neighborhood greenway will transition to a greenway trail and lead trail users along the southern side of the greenspace, where it will continue westward through DeKalb County owned property until it reaches the intersection of Briarcliff Road and Cliff Valley Way.

Overview

Distance: 4,752 Linear Feet (0.9 miles)

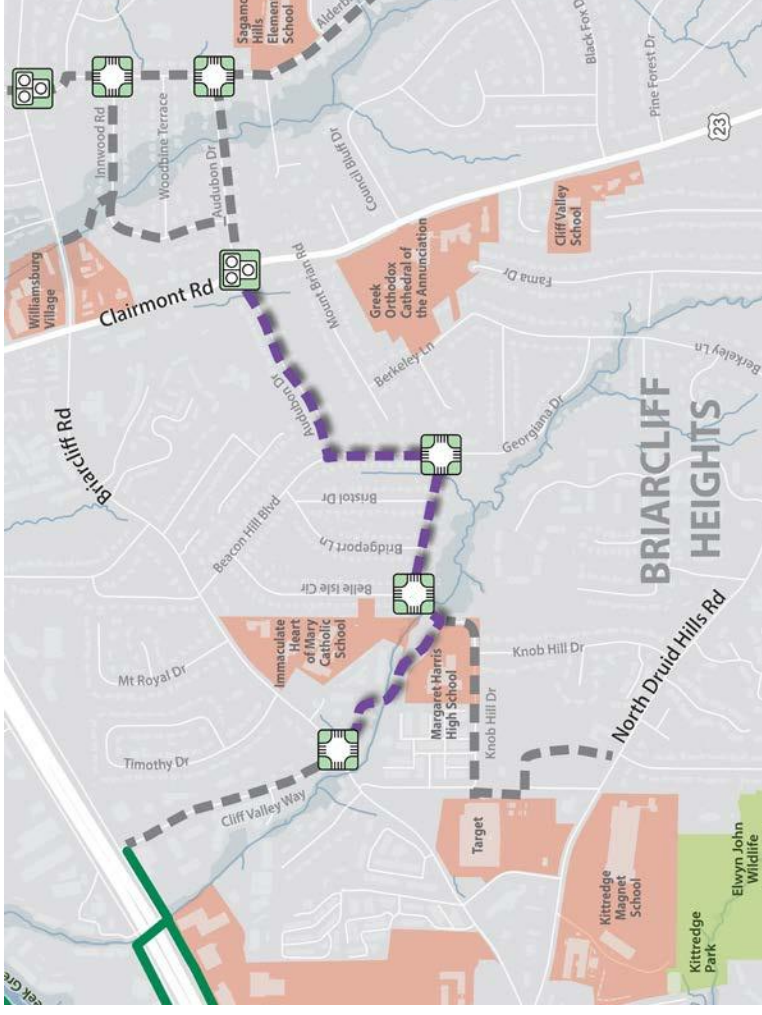
Begins: Audubon Drive and Clairmont Road

Ends: Briarcliff Road and Cliff Valley Way

Connecting Destinations: Briarcliff Heights Neighborhoods, Margaret Harris High School, and Immaculate Heart of Mary Catholic School.

Opportunities and Benefits

- Establishes bike/ped connectivity through Briarcliff Heights neighborhood via Calm Streets treatment within public right-of-way and Greenway Trail
- Achieves the connectivity goal of the project by connecting the neighborhood to schools and places of employment
- Sets the stage for future connectivity to the Peachtree Creek



Legend

- Segment 2
- Study Segments
- Existing Trail
- Future Trail
- Park
- Floodplain
- Point of Interest
- Calm Street Improvements
- Intersection Improvements
- Signalized Crossing
- Trail Crossing
- Roundabout Improvements

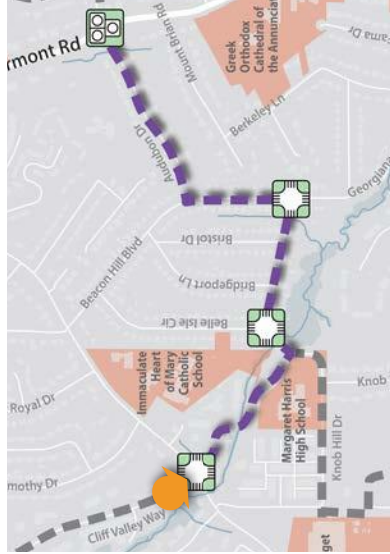
Greenway and other regional destinations.

Potential Challenges

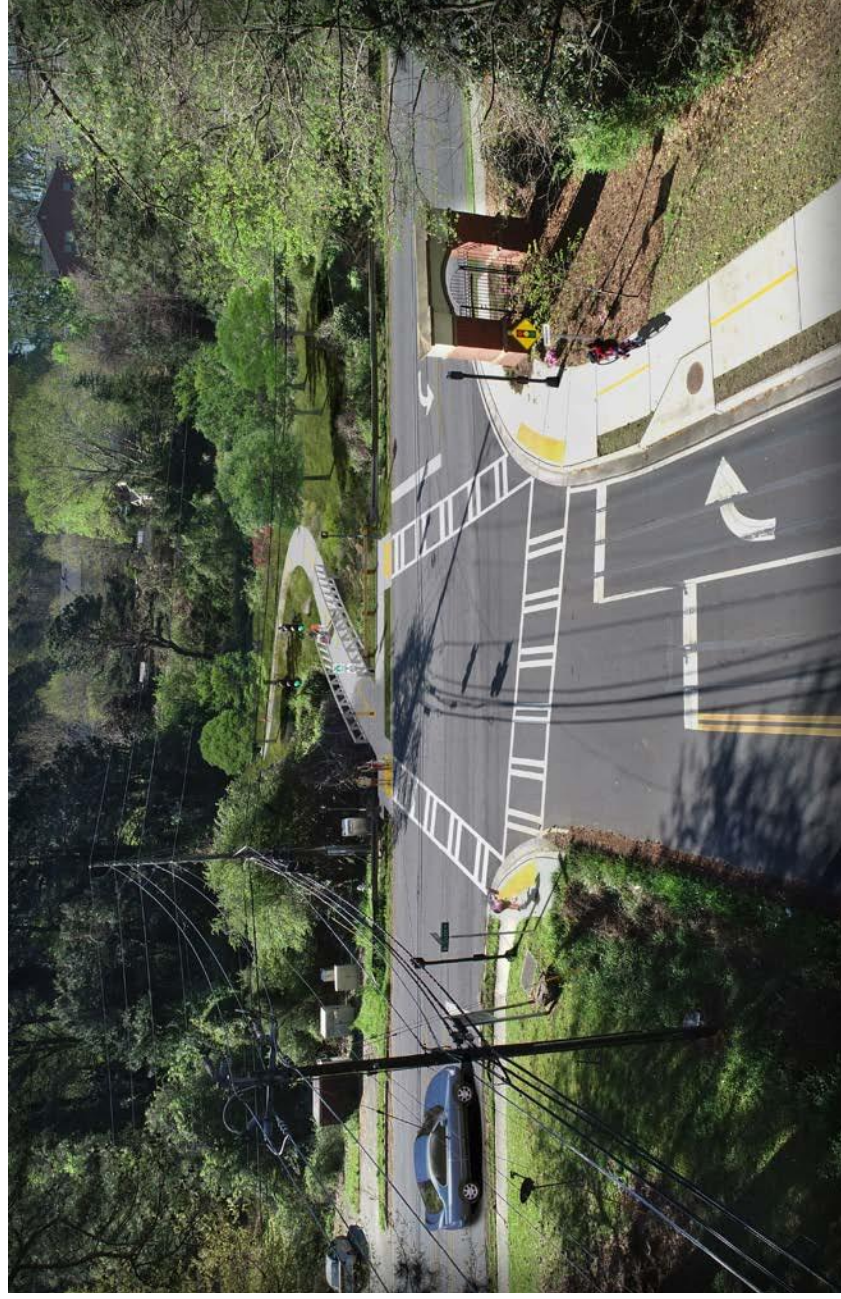
- Easement acquisitions are required from private property owners
- However, some opposition from residential property owners along Audubon Drive west of Clairmont.

Segment #2 | Proposed Intersection Upgrade

The vision graphic below depicts a 12' wide buffered sidepath along Cliff Valley Road that transitions to a 12' wide greenway trail South of Briarcliff Road. To safely connect these two sections of the trail, the intersection of Cliff Valley Way and Briarcliff Road is upgraded with new crosswalks and crossing signals.



Existing conditions at the intersection of Cliff Valley Way and Briarcliff Road facing Southeast.



Segment #3 | Cliff Valley Way to Peachtree Creek Greenway

Description

Segment 3 begins as a buffered side path at the intersection of Cliff Valley Way and heads North to the I-85 North, utilizing the existing Children's Health Care of Atlanta (CHOA) buffered side path and crossing under the interstate. This crossing already has a pedestrian hybrid beacons (sometimes referred to as High Intensity Activated CrossWalk (HAWK) beacons) installed and leads to an existing buffered side path that heads West along the I-85 South access ramp to Corporate Boulevard. A buffered side path would be added along the westside of Corporate Boulevard, passing the Salvation Army and leading users to the existing Peachtree Creek Greenway in Brookhaven. By building the network around the existing side paths underneath and on either side of the interstate, Segment 3 makes CHOA, a major destination and employment center, much more accessible to modes of transportation other than cars. The connection to Corporate Square, a large employment center located on Corporate Blvd, would also alleviate some pressure on the need for commuting to this destination by car.

Overview

Distance: 2,640 Linear Feet (0.5 miles)

Begins: Briarcliff Road and Cliff Valley Way

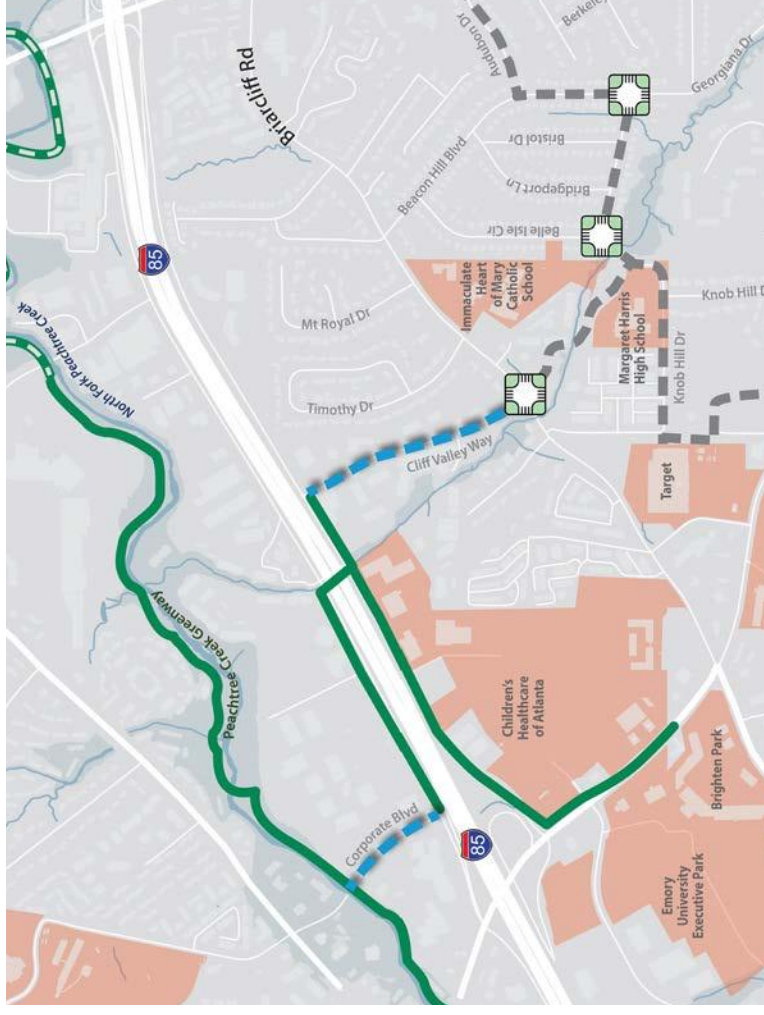
Ends: Corporate Blvd and the Peachtree Creek Greenway

Connecting Destinations: Peachtree Creek Greenway, Children's Healthcare of Atlanta (CHOA), Corporate Square, and the Salvation Army

Opportunities and Benefits

- Establishes bike/pedestrian connectivity to the Peachtree Creek Greenway via a buffered sidepath within public right-of-way
- Achieves the connectivity goal of the project by connecting to the existing Peachtree Creek Greenway
- Connects neighborhoods to places of employment at Children's Health Care of Atlanta (CHOA)

DeKalb County, Georgia



Potential Challenges

- Utility relocations on Cliff Valley Way
- Requires roadway adjustments to existing travel lanes along Cliff Valley Way

Legend

- Segment 3
- Study Segments
- Existing Trail
- Future Trail
- Park
- Floodplain
- Point of Interest
- Calm Street Improvements
- Intersection Improvements
- Signalized Crossing
- Trail Crossing
- Roundabout Improvements

Segment #4 | Williamsburg Village Connector

Description

Segment 4 begins as a neighborhood greenway heading west along Innwood Road from the intersection at Ravenwood Way back to Audubon Drive. A greenway trail starts to pick up at the west end of Innwood Road and heads north to Briarcliff Road through a Georgia Power easement. The utility corridor requires minimal acquisition from property owners while connecting Sagamore Hills residents to places of employment, entertainment, and restaurants in the future Williamsburg Village redevelopment. A new traffic signal and road connecting to the I-85 frontage road are being proposed by the redevelopment at Briarcliff Road. If approved, these amenities will provide an opportunity for the trail users to safely cross Briarcliff to the shopping center.

Overview

Distance: 2,617 Linear Feet (0.5 miles)

Begins: Innwood Road and Ravenwood Way

Ends: Briarcliff Road and Innwood Road

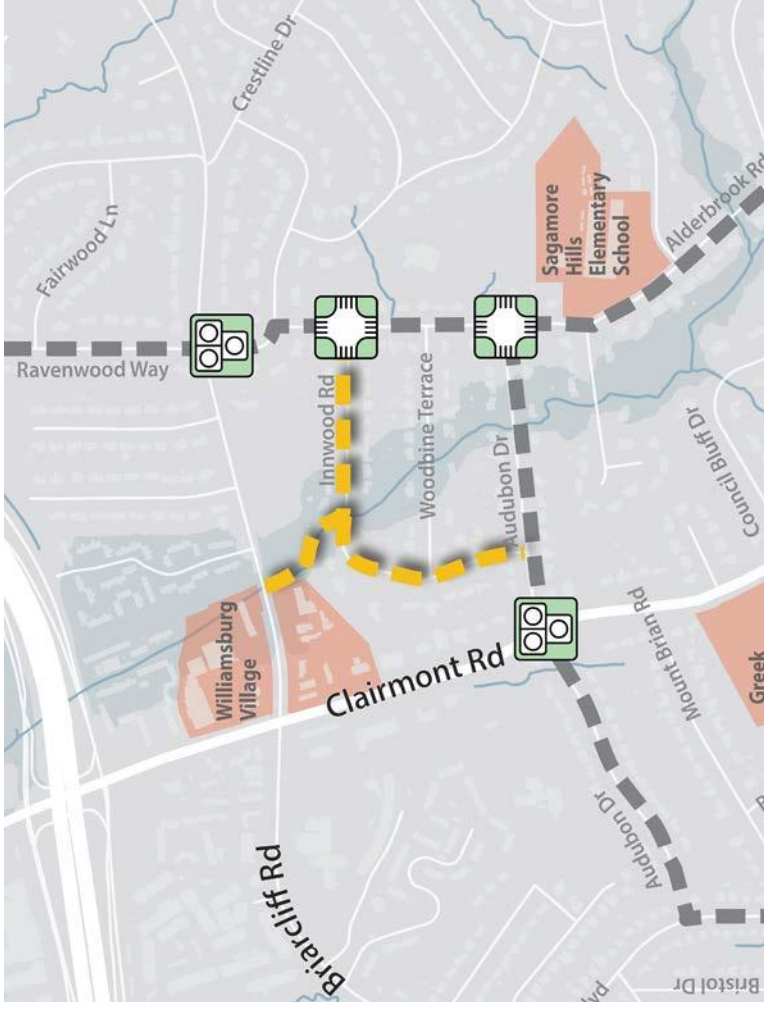
Connecting Destinations: Williamsburg Village, Audubon Briarcliff Apartments, and the Sagamore Hills Neighborhoods.

Opportunities and Benefits

- Establishes connectivity to the Sagamore and Leafmore neighborhoods via Greenway Trail and Calm Streets treatment within public right-of-way
- Achieves the connectivity goal of the project by connecting neighborhoods to places of employment, shopping centers and entertainment in Williamsburg Village.

Potential Challenges

- Easements required from property owners
- Agreements from Georgia Powers



Legend

- Segment 3
- Study Segments
- Existing Trail
- Future Trail
- Park
- Floodplain
- Point of Interest
- Calm Street Improvements
- Intersection Improvements
- Signalized Crossing
- Trail Crossing
- Roundabout Improvements

Segment #5 | Knob Hill Drive Connector

Description

Segment 5 begins as a neighborhood greenway heading west along Knob Hill Road from the east side of Margaret Harris High School. There is currently a narrow footpath that connects to the Target shopping center where Knob Hill turns north. Segment 5 proposes a small greenway section to make this connection safer and more established for residents of Briarcliff Heights neighborhoods and students and visitors of Margaret Harris High School. In addition to the connection at the high school, a route that borders a future development will allow trail users to easy access North Druid Hills Road where there is an existing pedestrian crossing at Holly Lane.

Overview

Distance: 3,000 Linear Feet (0.6 miles)

Begins: Knob Hill Drive at Margaret Harris High School

Ends: Target Shopping Center

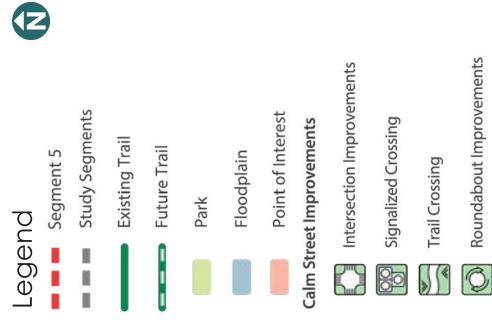
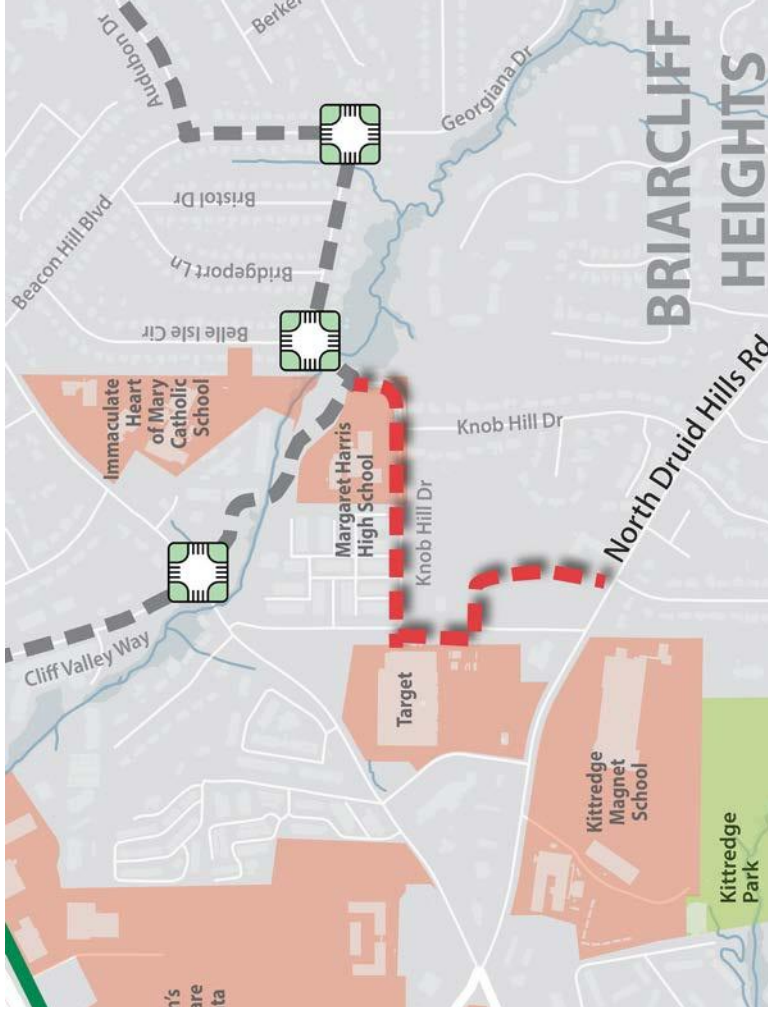
Connecting Destinations: Briarcliff Heights Neighborhood, Margaret Harris High School, Target Shopping Center, North Druid Hills Road

Opportunities and Benefits

- Establishes connectivity to Briarcliff Heights neighborhood via Calm Streets treatment within public right-of-way and Greenway Trail
- Achieves the connectivity goal of the project by connecting the neighborhood to places of employment and entertainment through the Target shopping center
- Provide opportunities for further connections to N. Druid Hills Road and Kittredge Park

Potential Challenges

- Easements required from private property owner





03

Implementation Strategy

03 Implementation Strategy

Recommended Implementation

Reviewing each proposed connectivity segments against the guiding factors is critical to ensure the feasibility of this project. Segment 1 has the most property stakeholder support and does not entail critical areas for real estate acquisition outside of existing right-of-way. Implementing Segment 1 as the designated calm street route will achieve the connectivity goal from the existing South Peachtree Creek Trail at Mason Mill Park to the future Peachtree Creek Greenway corridor.

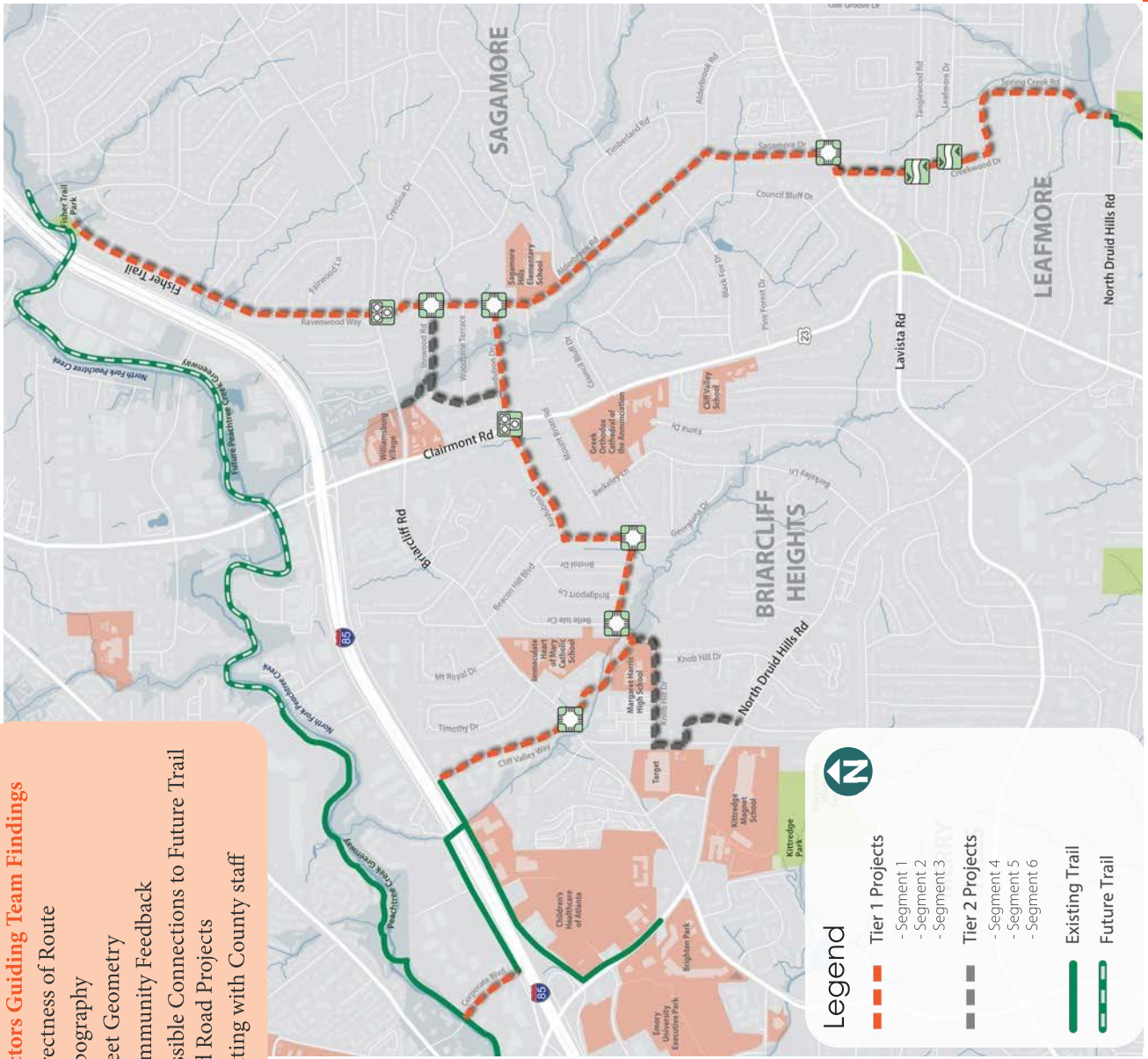
The study team received support for Segment 2 from residents in both neighborhoods for an improved bike/ped connection. However, some opposition was received from residential property owners along Audubon Drive west of Clairmont. This route is being identified as the only feasible and safe option for crossing Clairmont Road by utilizing the proposed pedestrian signalized crossing at Audubon Drive in the programmed GDOT Clairmont Road project.

Segment 3 was strongly supported by residents to connect the Leafmore, Sagamore, and Briarcliff Heights neighborhoods to the existing Peachtree Creek Greenway and CHOA. This segment has few non-residential stakeholders and will complete the connection to the existing Peachtree Creek Greenway.

By implementing Segment 1, 2 and 3, the County will establish the corridor to sync with the future GDOT project timeline, which will result in a continuous connection between all three neighborhoods within the study area. Further coordination with property owners should be taken into consideration prior to beginning design and engineering of each segment.

Factors Guiding Team Findings

- Directness of Route
- Topography
- Street Geometry
- Community Feedback
- Possible Connections to Future Trail and Road Projects
- Vetting with County staff



Preliminary Cost Estimate

The chart on this page shows the conceptual cost estimate summary for each segment identified in this study. This preliminary cost estimate includes Estimated Acquisition (RE), planning & engineering (P&E), and construction costs for the proposed trail.

Estimated Real Estate (RE) cost is based on similar land uses and like locations. The cost includes required due diligence such as environmental analysis allotment, appraisal of market value and legal costs associated with real estate transactions.

Estimated Planning and Engineering (P&E) cost includes the cost of surveying, designing for construction, permitting, bidding, and construction administration of the project.

Estimated construction costs are based on material and labor pricing from spring 2022 using South Peachtree Creek Greenway trail standards. Those standards include an average of 12’ wide concrete trail, landscape, structures and walls, boardwalks with a concrete deck and metal railing, trail amenities, signage, and calm street treatments. Estimated allowance for utility relocations is also included in the construction costs as noted in the summary.

Next Steps

- Present Study Report to County Commission for Feedback and Adoption
- Allocate acquisition and construction funding for prioritized segments
- Complete P&E for prioritized segments
- Review final plan and advance remaining segments for implementation

Cost Summary

Mileage	Trail Implementation Phases	RE	P&E	Construction	Total
3.2	Segment 1 - N. Druid Hills Drive to Fisher Trail Park	\$443,000	\$118,000	\$1,029,000	\$1,590,000
0.9	Segment 2 - Briarcliff Heights Connector	\$240,000	\$118,000	\$1,329,000	\$1,687,000
0.5	Segment 3 - Cliff Valley Way to PCG *	\$164,000	\$107,000	\$776,000	\$1,047,000
0.3	Segment 4 - Williamsburg Village Connector	\$133,000	\$144,000	\$1,105,000	\$1,382,000
0.6	Segment 5 - Knob Hill Drive Connector * **	\$378,000	\$89,000	\$504,000	\$971,000
5.5	Total	\$1,358,000	\$576,000	\$4,743,000	\$6,677,000

* denoted segment's construction cost estimate includes utility relocation.

** 0.2-miles of Segment 5 will be constructed by a private development, and therefore was not included in this cost estimate.

04

Appendix

[South Peachtree Creek Trail Study Public Meeting #2](#)

Wednesday, May 11, 2022

From 6:00 to 7:30 P.M.

Comments:

- Steve Clowse: I live near Fisher Trail Park and would LOVE connectivity all the way down to the South Creek Trail at N. Drive Hills and Spring Creek. I would prefer as much greenspace as possible vs calm streets. Clearly, the Sagamore Hills residents on the easement at least the ones who showed up are against this! Are there any options? Is the easement public or private land? Unnecessary, calm streets would work. I hate for that greenspace to be unused and it would be safer for those who use it.
- Martha Richards: There needs to be another meeting soon and it needs to be better publicized. Hundreds of homeowners will be affected by this and their numbers were not well represented due to the lack of notice.
- Alice Richards: If there would indeed be hundreds of people on these trails, why would I want to invite them to my quiet neighborhood? We have deer, turkey, foxes, raccoons living in these utility corridors. I am opposed to disturbing their habitat. The paths don't lead to anyplace, not the one along Alderbrook anyway!
- Gail Richman: You may wish to contact Theresa Same Park, She heads up a cross neighborhood group who represent neighborhood associations immediately adjacent to South Peachtree Creek. We meet as a group and can provide feedback.
- Patrick Dean: I am very excited about the prospect of improved connectivity within the Peachtree Creek water shed. I support the utilization of existing power and sewer as they provide effective and safe liner access for a trial network.
- Carole Ashworth: I own the property in Tanglewood through which the Georgia power utility easement gives. I am very concerned with the impact that a path so close to my home will have 1,000 people a day walking just feel away from my home is alarming. Both from a security issue, aesthetic and the effect on my property value. Am very opposed!

- Gbroussard53@aol.com: Traffic calming measures on Alderbrook Rd to allow pedestrians, bikes, and cars to calmly/slowly get from La Vista to connector to South Peachtree Path. Not using the utility right of way. I live on Alderbrook and back up to the utility pathway. It's not really suitable.
- Barbara Vargas: I'm highly concerned about Fisher Trail to PCG safety of cyclists and pedestrians. Cars speed and traffic very heavy on this road. Off street (sidewalks, etc.) improvements are imperative.
- Cheryl Brown: Have received information on this project and there is no excuse. Beacon Hill is a cut through with a blind hill right before the proposed path. Meeting was very boring.
- Doallin: The team leading the meeting was not interested in input that did not support their AGENDA! We do not want a path in the ROW in Council Bluff / Sagamore Hills. Clairmont Road is being redeveloped incorporate a path there.
- Joe Peragine: We don't want a path in the Sagamore Hills ROW.
- Tyler and McKenna Law: We are excited and in full support! This is directly in our backyard. We have no sidewalks but a lot of traffic on our street. Driving to South Peachtree Creek isn't sustainable for us. Current usage of easement isn't equal or fair. Our rear neighbors have hijacked the entire area. Would prefer a trail (GP doesn't care)! Our yard backs up to utility corridor and we are in favor of this. Concerns of safety are unfounded. There is a public street in front of all of our houses. What difference would a trail make (those are the concerns online). We will consider moving to a home near a community path like this one for our next home.
- Sinead Snow: It is hard to get behind new infrastructure when the current is crumbling. My home would be on either the off road or on road connections and both would be located near sinking property. Seems dangerous to build on top of that if the county is refusing to up keep and repair it.

- Alexandria Yang: I am a regular user of the S. Peachtree Creek Trail. I enter from the Medlock Park entrance. I realize that the Latham entrance will offer some relief to this but I think it's very important to do a study on the east bound PM traffic on N Druid Hills Rd. It's very dangerous to make turns in and out of the neighborhood (not calm). Please take a look at this with the project. We are PRO path (yay)!
- Doug Jonkheet: I support the new off-street connections in general and specifically at Cliff Valley and at Council Bluff.
- Paul Whitaker: Complete Peachtree Creek Greenway! Extend South Fork Pt Creek trail north up powerline easement to Peachtree Creek Greenway!
- Ashlie Shepard: While it's being said there is "coordination" with GDOT plans, it was mentioned of a "round-about" at Audubon during tonight's meeting. The GDOT is planning a "round-about" at the Greek church, 2 roundabouts within 0.1 miles? Not in support of crossing Clairmont at Audubon or the off-road path at Audubon dead end through to Beacon – Safety! Safety! Concerns: Safety, crime trash. Keep the community informed about meetings! So far, outreach minimal.
- John McCauley: As a dad for a young family in Sagamore Hills on Black Fox Dr (one of the proposed paths) I am fully in support of bringing PATH through our neighborhood. I often don't feel safe walking in let alone between neighborhoods with my kid. Traffic calming and dedicated path would make Sagamore Hills safer and more welcoming. There are lots of families who feel the same.
- Dawn Lanier: Please consider a side path from mistletoe to Spring Creek on NDH Rd. Are you engaged with overall plan for NDH Rd (apparently a 10-year plan from I-85 to 78 and beyond)?
- Scott Cooper: Love more walking / riding opportunities. We live on power line easement and are supportive of PATH there. I am happy to get more involved.
- Carolyn Law: Please contact me with information @ future meetings: carolynlaw@mac.com. Board Member, Safety – Briarcliff Woods Civic Association.

- Helena Pachon: I work at Emory University and would love to walk through N. DeKalb more to Medlock Park. I sometimes walk between work and home, but I have to walk on major streets like N. Druid Hills Rd. Would also appreciate paving of "Trail" on N. Druid Hills Rd. between Bireit Rd. and Druid Walk to allow safe walking to N. DeKalb mall.
- Linda Harris: Unlike a comment from a Leafmore person in the meeting tonight, not all of Leafmore is against the PATH. That was a stretch. I love the thought of access through Leafmore and Sagamore to I-85.
- Sally Sears: Extend from Clairmont to Houston Mill along sewer lines.
- Shirley Lee: Please put a safe sidewalk from Spring Creek Road up to Clairmont Road so that people in the creek park neighborhood could safely walk the 1 mile to Toco hills shopping center.
- Vickie Berg: Very concerned about PATH coming through neighborhoods and disturbing the peace and quiet of a residential home. PATH directing people into neighborhoods also brings up the concept of safety. I would prefer sidewalks along existing roads provides transportation and safe travel without creating neighborhood issues.
- Robert Pich: We live in Briar Cliff Woods. We use the Peachtree Greenway. Every weekend to run. We got there by car. We also like to visit Medlock as well. We get there by care. I like that a plan for Fischer Trail is part of the PATH. I like to have parking at the major parks.
- Simone Rosa: As a resident of Sagamore Hills, our Civic Associate Priest and I (others) never heard of this until this meeting. We found out about this from a next-door post. While I like the idea of traffic calming and sidewalks on our streets with bike lanes, I agree that going through the yards of our neighbors and our Leafmore neighbors. I've never used an Atlanta Greenway because I'd have to drive to a trailhead and most have no parking.
- Gail Richman: Can you provide any study results regarding the increase or decrease of crime (robbery break-in, theft) in neighborhoods having off-street paths? North Druid Valley Home Assn. NDUHA. Includes Latham Rd.

- Laura Zipperer: The Latham Rd. connector is not 100% approved by our neighborhood (N. Druid Valley). It would depend on type of construction (your picture shows concrete which would not be preferable in the floodplain) and parking on Latham that can't handle it. Please reach out to our neighborhood directly.
- Elizabeth Garalis: Now that you know 1715 and 1707 Beacon Hill Blvd NE is privately owned, are you going to remove Audubon Dr. NE as a "proposed" PATH to Beacon Hill NE? Using Audubon Dr NE is redundant to Berkeley Lane. Removing trees will cause more issues with sun and we will lose our owl. Utilize Briar Cliff intersection vs Audubon Dr. NE.
- Bonnie and Gary Bradley: Now that you know 1707 Beacon Hill Blvd. and 1715 Beacon Hill Rd. is privately owned are you going to remove the "yellow" proposed path through our yard/home?
- Marian Costarides: The quality of life in our neighborhood, Audubon Drive, the dead end, is currently good. Several of us that live near the dead-end part of Audubon want to leave things as they are. We do not want lots of traffic bikes, outside of our neighbors, etc.) to be in our neighbor. Primary concern is safety and privacy! Do not want connectivity or Audubon Drive! I'd rather diver to a park than have outsiders in our quiet neighborhood.
- Andrea Kennedy: I live in Audubon Dr. NE and I don't want to connect my street, Beacon Hills. Also, I don't think taking other people's property is any good. You people don't have respect for trees and animals. Fix street potholes that's what we need!
- Alex Kennedy: As an Audubon Drive NE resident I am very concerned with the proposed cross through between Audubon and Beacon Hill. This is private property not on an easement. Also, there is safety issues putting a cross through there is no safe way to cross Clairmont.
- Whitney McGinnis: Would love safe way to cross. North Druid Hills at Mistletoe to get to new mall development.